



**DESIGN REVIEW BOARD AGENDA
NOTICE OF MEETING**

*Monday, October 30, 2023 at 4:00 p.m.
Council Chambers, 2nd Floor City Hall, 199 Queen Street
Live streaming: www.charlottetown.ca/video*

1. **Call to Order**
2. **Declaration of Conflicts**
3. **Approval of Agenda** – Approval of Agenda for Monday, October 30, 2023
4. **Adoption of Minutes** - Minutes of Design Review Meeting on, Monday, June 19, 2023
5. **Business arising from Minutes**
6. **Report:**
 - a. **17-19 Richmond Street (PID # 339739) Laurel**
Request to review an exterior design proposal for a new four (4) storey multi-unit development in the Downtown Neighbourhood (DN) Zone.
 - b. **156 Prince/199 Grafton Street (PID # 342790) Laurel**
Request to review an exterior design proposal for an eight (8) storey multi-residential building in the (DMUN) Downtown Mixed Use Neighbourhood Zone. The applicant has requested to increase the already approved 6 storey multi-unit building to 8 storeys. This proposal will exceed Bonus Height for the (DMUN) Downtown Mixed Use Neighbourhood Zone.
 - c. **40 Riverside Drive (PID #839332) Sonia**
Request to review an exterior design proposal for new waste to energy building in the Light Industrial (M-1) Zone. The applicant has requested to
 - Increase the maximum permitted height from 12m to approximately 35m for a building in the (M-1) Light Industrial Zone;
 - Increase the maximum height of rooftop mechanical equipment from 3m to 15m above the maximum permitted height;
 - Reduction in the minimum required setback of the rooftop mechanical equipment from 3m to 2m from the roof edge.
7. **Introduction of New Business**
8. **Adjournment**

PLANNING AND HERITAGE: DESIGN REVIEW BOARD MINUTES
MONDAY, JUNE 19, 2023, 4:00 P.M.
COUNCIL CHAMBERS, 2nd FLOOR, CITY HALL
Live Streaming: www.charlottetown.ca/video

Present: Deputy Mayor Alanna Jankov, *Chair* Aaron Stavert, RM
Brian Gillis, RM
Sharon Larter, RM
Jacqui Scaman, RM*
**participated via teleconference*
Mary Nicholson, RM

Also: Eleanor Mohammed, CAO** Robert Stavert, IO/AA**
***left at 4:39 pm* ***minute taker*
Laurel Palmer Thompson, PII

Also in attendance:
(applicants)

Regrets: Councillor Julie McCabe Kenneth McInnis, RM
Councillor Norman Beck Kris Fournier, RM
Mayor Philip Brown

1. Call to Order

Deputy Mayor Jankov called the meeting to order at 4:07 p.m.

2. Declaration of Conflicts

Deputy Mayor Jankov asked if there are any other conflicts. There being none, moved to the approval of the agenda.

3. Approval of Agenda

Moved by Sharon Larter and seconded by Brian Gillis, that the agenda for Monday, June 19, 2023, be approved.

CARRIED

4. Adoption of Minutes

Moved by Sharon Larter and seconded by Aaron Stavert, that the minutes of the meeting on Monday, April 17, 2023, as circulated, be approved.

CARRIED

5. Business arising from Minutes

There was no business that arose from the minutes.

6. 308-312 Richmond Street (PID #339598)

Request: Review an exterior design proposal for a new three (3) storey townhouse in the Downtown Neighbourhood (DN) Zone.

Property Information: Reference: Downtown Neighbourhood (DN) Zone in the 500 Lot Area.

Presenter: Laurel Palmer Thompson

Reference: Design Review Board Package

Summary:

- The original façade drawings and floor plans submitted on August 8, 2022, was submitted to Peter Fellows, Design Reviewer, for comments.

Design Review Board

June 19, 2023

Page 2 of 3

- While the design reviewer appreciated the proposed design and use of modern materials, it does not fit the neighbourhood and does not support the design. The major design flaws are as follows:
 - Inappropriate Scale (4 storeys)
 - Inappropriate Roof lines (flat roof)
 - Inappropriate Fenestration (use of high modern triple vinyl windows on street façade)
 - General disregard for the Streetscape (27% of properties within the block are heritage properties)
- The design reviewer provided sketches and suggestions on how the design flaws could be addressed.
- The applicant and architect reviewed the suggestions and resubmitted a new design incorporating the design reviewer's suggestions.
- The new proposal included a gabled roof, double hung windows and balconies using more traditional materials, and removed the 4th storey roof top terrace.
- Since the revised design addressed most of the Design Reviewer's comments, the revised drawings were not sent back for a second design review.
- Planning staff consulted with the City's Heritage Officer to review the submissions and design reviewer's comments and agreed that the revised design meets the suggestions of the design reviewer.
- One additional request was to change the garage door to blend with the architectural style of the building. The architect has provided an image of the proposed garage door.
- Staff felt that the revised design proposal met the recommendations from the design reviewer and would fit the streetscape within the neighbourhood. The design also met the intent of the Design Review guidelines outlined in the Zoning & Development Bylaw.
- Staff recommended that the revised design submitted on March 4, 2023, and garage doors on May 11, 2023, be approved.

Discussions:

- Are there any site plans or setbacks submitted? *No, staff has not received a site plan yet.*
- The setbacks are important to determine the proper materials and opening sizes to ensure that it meets the National Building Code. It is also difficult to review without a full package being submitted.
- Staff mentioned that the materials used would be wood and metal sidings. The setback requirements will be reviewed by staff to ensure that it also meets the Zoning & Development Bylaw requirements.
- What will happen if the building code determines that the materials do not meet the code? *The Building Code requirements should be satisfied.*
- How do you draw the line between not looking like a heritage building and not looking too modern? *New developments should not result in historic mimicry. The original design was attractive but is just not appropriate in the area where it is being proposed.*
- It looked like the suggestions by the design reviewer on windows were not considered.
- Members of the board felt that there wasn't enough information in order to make a decision and were looking to reject the request and have it sent back to the design reviewer. It was later suggested that the application be deferred and sent back to the design reviewer.
- Staff noted that Peter Fellows has since retired. A new design reviewer would have to be secured to review the revised application.
- The Bylaw outlines a list of requirements in order for an application to proceed to design review. It was suggested that these requirements be provided before it is submitted back to the board. If the requirements for a full submission package are not met, it was suggested that the application be rejected.

Motion:

Moved by Brian Gillis and seconded by Aaron Stavert, that the design proposal for a new three (3) storey townhouse in the Downtown Neighbourhood (DN) Zone located at 308-312 Richmond Street (PID #339598), be deferred until the following conditions are met:

- **Reconsider the application based on a complete application package as outlined in the Zoning & Development Bylaw;**
- **Reconsider suggestion from Peter Fellows (original design review); and**
- **Submit complete application for design reviewer's comments.**

MOTION CARRIED

(5-0)

7. New Business

There was no new business introduced.

8. Adjournment

Moved by Sharon Larter and seconded by Aaron Stavert, that the meeting be adjourned. The meeting was adjourned at 4:43 p.m.

Deputy Mayor Alanna Jankov, Chair

Minutes prepared by: Ellen Faye Catane

TITLE: <p style="text-align: center;">DESIGN REVIEW FILE: DESIGN-2023-30-OCTOBER-6A 19 RICHMOND STREET (PID #339739) OWNER: 103040 PEI INC. (JACKIE AND PHILIP O'HALLORAN)</p>		
MEETING DATE: October 30, 2023		Page 1 of 5
DEPARTMENT: Planning & Heritage	ATTACHMENTS: <ul style="list-style-type: none"> A. Design Review Original Submission (June 27, 2023) B. External Design Reviewer Comments (August 8, 2023) C. Revised Design Reviewer Submission (September 12, 2023) D. Final Design Submission (October 24, 2023) 	
SITE INFORMATION: Ward No: 1 Queens Square Existing Land Use: Vacant lot previously occupied by Irwin Printing Official Plan: Downtown Neighbourhood Zoning: Downtown Neighbourhood Zone (DN)		

RECOMMENDATION:

The Planning & Heritage Department encourages the Design Review Board to approve the proposed revised building design as per the Design Reviewer’s recommendations and submitted drawings of October 24, 2023 (Attachment D) for the four (4) storey, multi-unit development located at 19 Richmond Street (PID #339739).

BACKGROUND:

Request

The applicant being the property owner has submitted a design for a proposed four (4) storey multi-unit apartment building to be constructed at 19 Richmond Street (PID #339739) which is the site of the former Irwin Printing Building. The request is to approve the design proposal.

By-law Requirement.

As per Section 7.1 of the Zoning and Development By-law, new buildings and building additions exceeding 215 square feet in size that are found within the 500 Lot Area are required to undergo the Design Review process. This is to ensure that the architectural design of proposed development within the 500 Lot Area maintains a higher quality of design and is constructed with a consistent type and quality of materials. From a design perspective, it is important to ensure that

new construction within the 500 Lot Area “is a product of its time” which compliments and is compatible with existing buildings on the streetscape and surrounding properties.

ANALYSIS:

On June 27, 2023, the applicant submitted concept façade drawings, floor plans and elevations for the proposed multi-unit apartment building (Attachment A). Planning staff sent the proposed design package to an independent design reviewer, Greg Munn, AAPEI, to perform a design review.

On August 8, 2023 Mr. Munn submitted his formal review as per Attachment B which indicated the following main points: ***Please note for clarity sake where the Design Reviewer refers to 17 Richmond the correct address is actually 19 Richmond. Also, number 15 Richmond is not under Design Review but was included for context as it went through Heritage Board approval.**

MASS AND SCALE

RECOMMENDATIONS: To address the vertical vs. horizontal issue with the design, it is recommended that the vertical nature of the protruding center mass is broken with the continuation of the datum line across it, or some other solution to downplay the vertical nature of the massing. There are several ways to address this issue and I will leave the design to the architect, but would suggest the vertical/ horizontal play on the East side elevation is more successful, as the datum divides the vertical elements (see walls at grid lines 3 and 4, East Elevation). The row house to the east is much wider than the proposed new building, and therefore, widening the planes of the proposed building would make it blend in better with its neighbours. Although the height of the building is within the 40' height limit, the proposed building is four-storey tall. As noted, this is not allowed in the DN zones of Charlottetown. It is recommended that the building design is revised and presented again as a three-storey building.

PORCHES AND ENTRIES

RECOMMENDATIONS: Design a level landing area at the front doors at least 5' deep, and include a ramp with rails no more than 12:1 or a sloped walkway great that 1:20 if handrails are not desirable. If any steps are included in the design, include handrails, even though the height does not require them according to NBCC.

WINDOWS

RECOMMENDATIONS: According to 7.5.4 of the Zoning and Development Bylaws, sliding doors are not permitted on the front façade of a building within the 500 Lot area. Therefore, these will not be acceptable as presented in the South Elevation, but what is portrayed in the 3D imagery is permitted. Please clarify that the French door option will be installed.

MATERIALS AND FINISHES

RECOMMENDATIONS: It is recommended that the metal Weathered Cedar Cladding is not used on the building. To compliment the period houses flanking the new construction, it is recommended that the palette is simplified. The other three sides of the building are actually more in keeping with the aesthetic of the other buildings on the street as the palette is simpler and the mass of the building is less sub-divided.

SUMMARY

17 Richmond Street: The design The number of floors proposed for the new building is a violation of the Lot 500 regulations for a DN zone. It is my opinion that the fourth floor on the street face will adversely affect the scale by visually standing above the other buildings on the street, interrupting its rhythm and scale. Although the building is within the height allowed, a 40' vertical wall will look like it stands twice as tall as the existing 20' walls flanking it, and may create a canyon between it and the 30' wall across from it. The issue of massing and scale is more than height and length. It's also, how much sky can you see when you are standing in front of it. Therefore I cannot recommend building to be built as designed, and that the design should be reconsidered as to how it meets the street.

In reviewing the proposal, the design reviewer was asked to consider the 10 Guiding Principles of the 500 Lot Area against the proposed development. Overall, in the initial review the design reviewer raised concerns about building height in the Downtown Neighbourhood Zone (DN) and indicated that he could not recommend approval of the building with a 40 foot vertical wall against the streetscape. The design reviewer did indicate that a 4-storey building was not permitted in the (DN) Zone. However, this was incorrect as the regulations in the (DN) Zone have been changed to allow a 4-storey building at 39.4 feet provided it is in keeping with the scale and rooflines of other buildings along the street. The minimum and maximum allowable range in height in the Downtown

Neighbourhood Zone (DN) is between 24.6 feet and 39.4 feet. The purpose of this range is to keep the height and scale of buildings in general alignment with one another so they appear harmonious along the streetscape.

After receiving comments from the design reviewer, staff forwarded Mr. Munn's report to the applicants and their architect. Both the applicants and their architect agreed to the suggested changes and revised the drawings to incorporate the suggested changes by the design reviewer. The fourth storey was also stepped back to incorporate a 4 foot step-back along the front façade as per Attachment C. The revised drawings were sent back to the design reviewer and it was suggested by the reviewer that a 4 foot step-back was not adequate to hide the fourth storey from adjacent buildings when viewed from the centerline of the street as per the following comments,

"The following are my thoughts regarding the resubmitted design for 15-17 Richmond Street. The material palette has been simplified and is clearer and more in keeping with the aesthetic of the street, which is positive. Also, the facade is simpler with a bigger emphasis on the horizontality of the Building and is appreciated. However, the intent of the setback was to make the building seem to be three floors as viewed from the street, so it does not loom over the neighbouring buildings. The proposed setback looks to be about 4', which means it will still be visible and prominent looking at the building from the street and does not meet the intent of the request. I feel this setback is not sufficient to satisfy the intent of the Bylaws and will still be too tall for the street. The Preferable the setback would align with the peaks of the neighboring houses. or at least enough so that the fourth floor is not visible from the center of the street."

The design reviewer's comments were sent back to the designing architect and the fourth storey was stepped back to 11 feet from the front façade of the building (see Attachment D). The design reviewer's final comments on the proposal with an 11 foot step-back were as follows:

"The scale and perceived height of the revised proposal feels right within the context of the neighbouring houses, and I am satisfied with the fourth floor stepback. I would recommend approval of this revised design."

Given that the designing architect revised the drawings to address the design reviewer's comments on architectural style of the proposed building and that the design reviewer has given approval on the revised drawings, planning staff are of the opinion that the revised design proposal meets the recommendations as put forward by the design reviewer and fits appropriately with the adjoining buildings along the streetscape.

Although the proposed building is higher than the 3.5 storey buildings along the street, the design which includes the step-back is sensitive in terms of infill development. Overall, the application

has met the intent of the Zoning & Development By-law and the 10 Guiding Principles of the 500 Lot Area Development Standards and Design Guidelines in terms of Design Review. Planning staff therefore agree with the recommendations as outlined in Mr. Munn's Design Review Report (attached) and his subsequent comments that have been included within this report.

CONCLUSION:

Planning staff recommend that the Design Review Board approve the proposed revised building design submitted October 24, 2023 (Attachment D) for the four (4) storey, multi-unit development located at 19 Richmond Street (PID #339739).

PRESENTER:



Laurel Palmer Thompson, RPP, MCIP,
Planner III

MANAGER:



David Douglas Gundrum, RPP, MCIP
Manager of Development Planning

Attachment A

Design Review Original Submission (June 27, 2023)

File: DESIGN-2023-30-OCTOBER-6A

19 RICHMOND STREET (PID# 339739)

OWNER: Jackie and Phillip O'Halloran



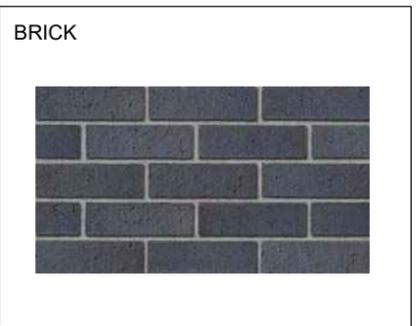
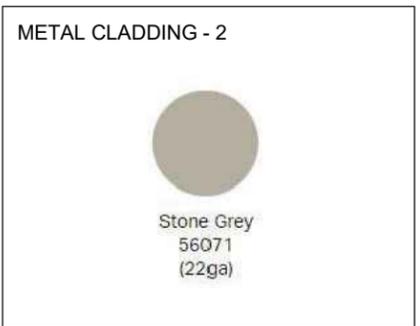


VIEW ALONG RICHMOND STREET 1



VIEW ALONG RICHMOND STREET 2

PRELIMINARY ONLY NOT FOR CONSTRUCTION



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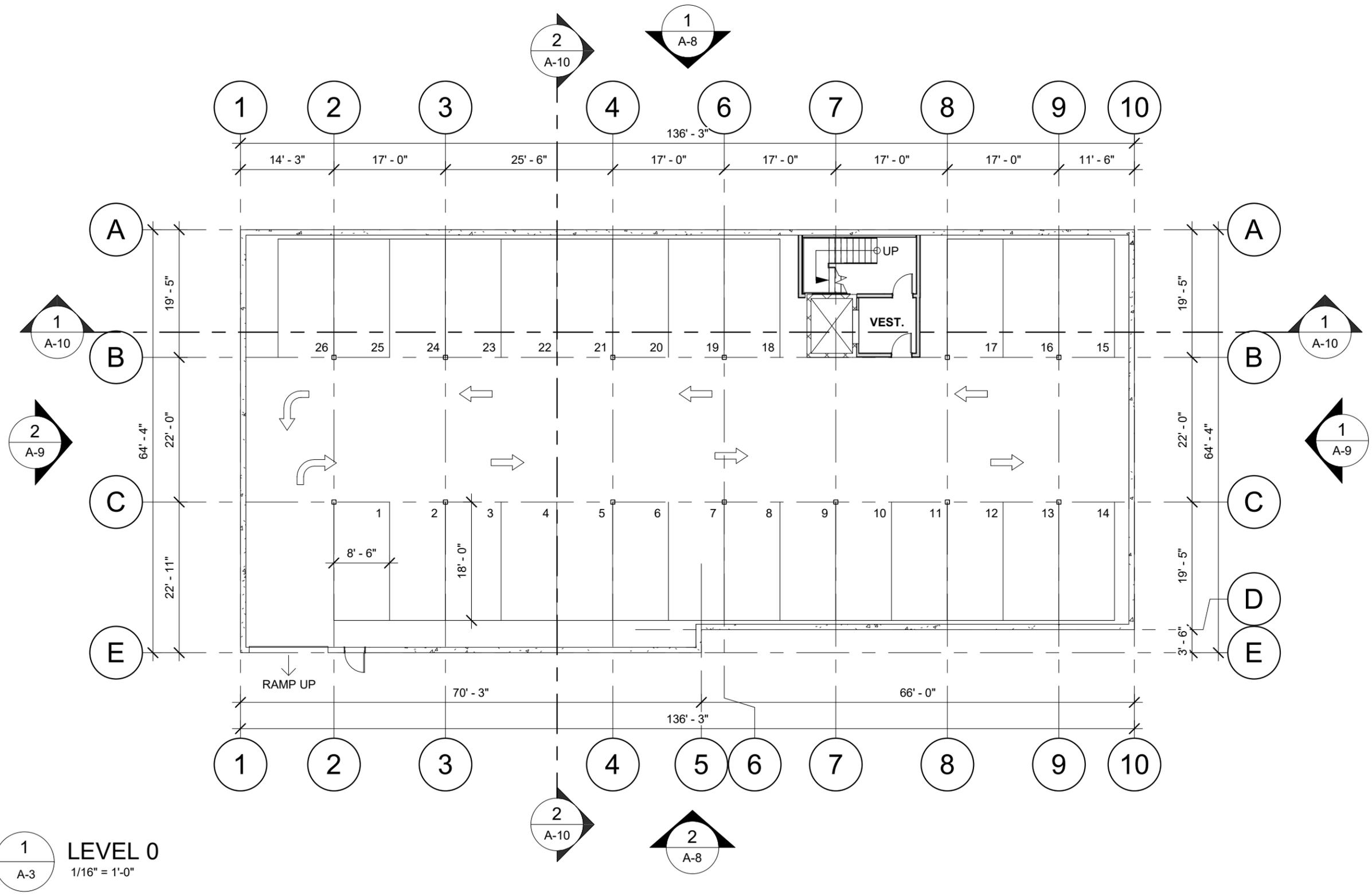
Client:
Jackie & Phil O'Halloran

Project Title:
**Richmond Street Urban
Housing Infill**

Sheet Title:
Exterior Materials

Date: 06/15/23
Drm By: JVJ Chk By: SDM

Project Number:
231024
Drawing Number:
A-2



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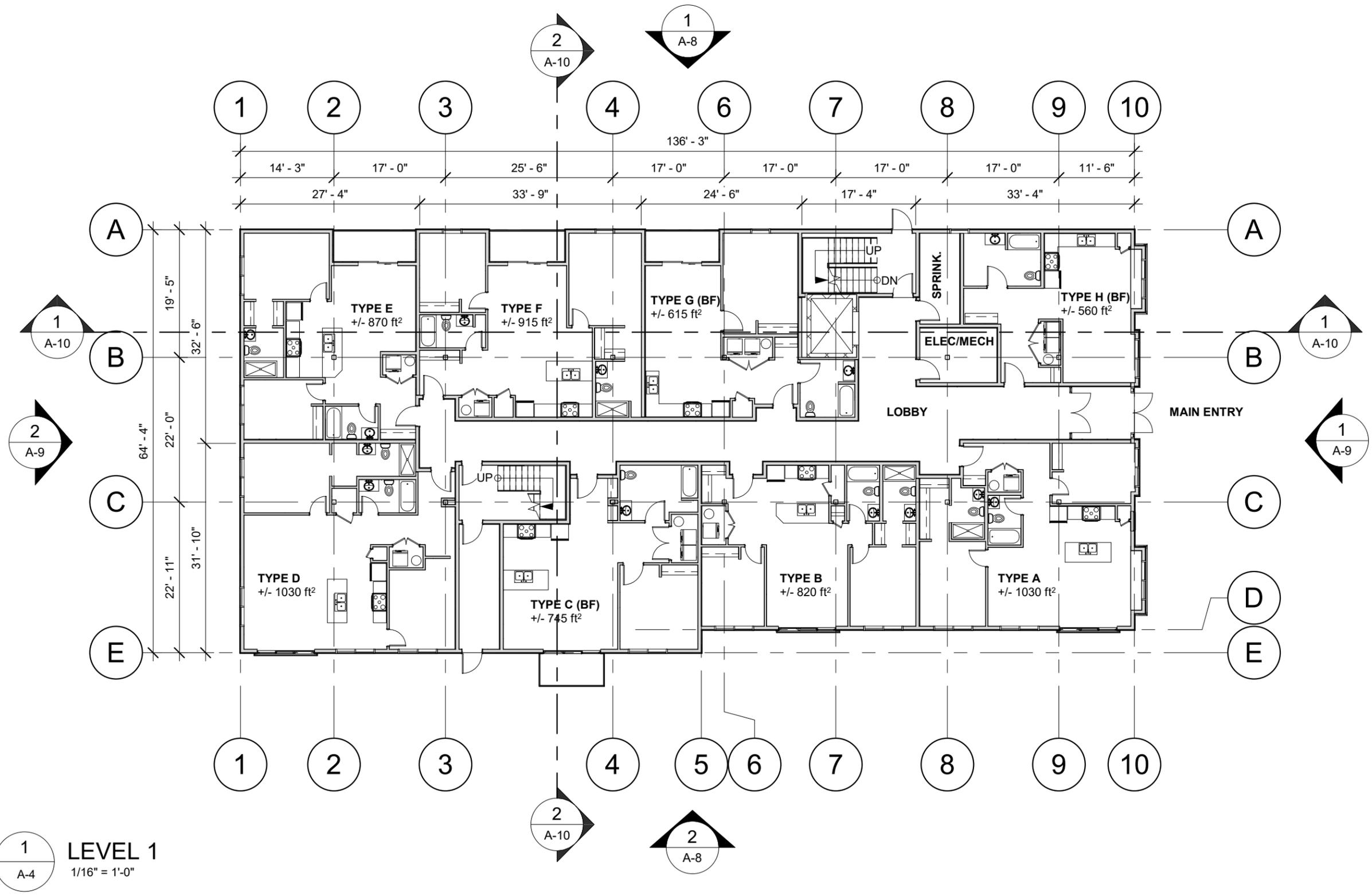
Client:
Jackie & Phil O'Halloran

Project Title:
**Richmond Street Urban
 Housing Infill**

Sheet Title:
Level 0 Floor Plan

Date: 06/15/23
 Dwn By: JLA Chk By: SDM

Project Number:
231024
 Drawing Number:
A-3



1
A-4
LEVEL 1
1/16" = 1'-0"

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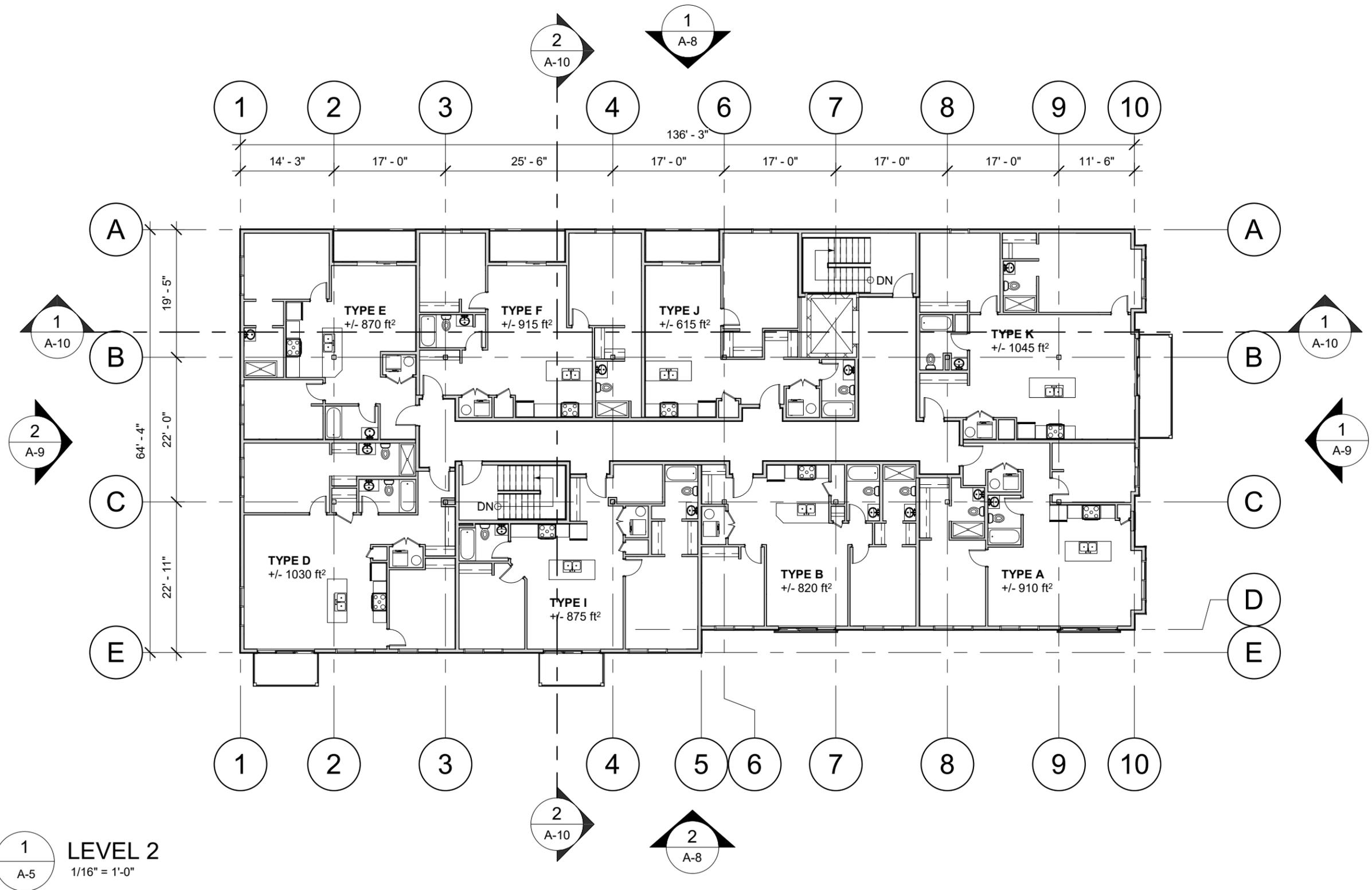
Client:
Jackie & Phil O'Halloran

Project Title:
**Richmond Street Urban
Housing Infill**

Sheet Title:
Level 1 Floor Plan

Date: 06/15/23
Dwn By: JLA Chk By: SDM

Project Number:
231024
Drawing Number:
A-4



1
A-5

LEVEL 2
1/16" = 1'-0"

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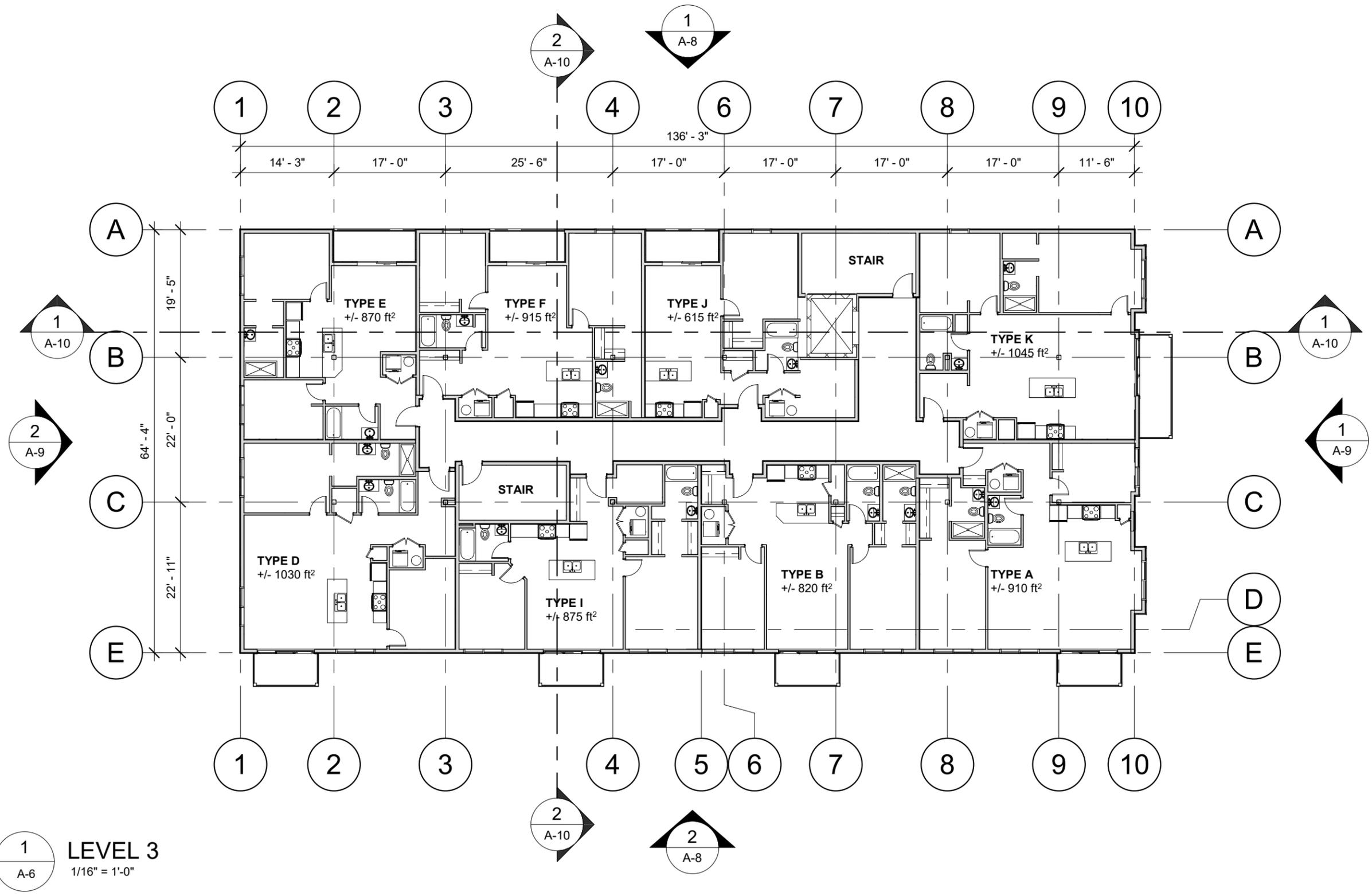
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Client:
Jackie & Phil O'Halloran

Project Title:
**Richmond Street Urban
Housing Infill**

Sheet Title:
Level 2 Floor Plan

Date: 06/15/23
 Drn By: JLA Chk By: SDM
 Project Number:
231024
 Drawing Number:
A-5



1 LEVEL 3
A-6 1/16" = 1'-0"

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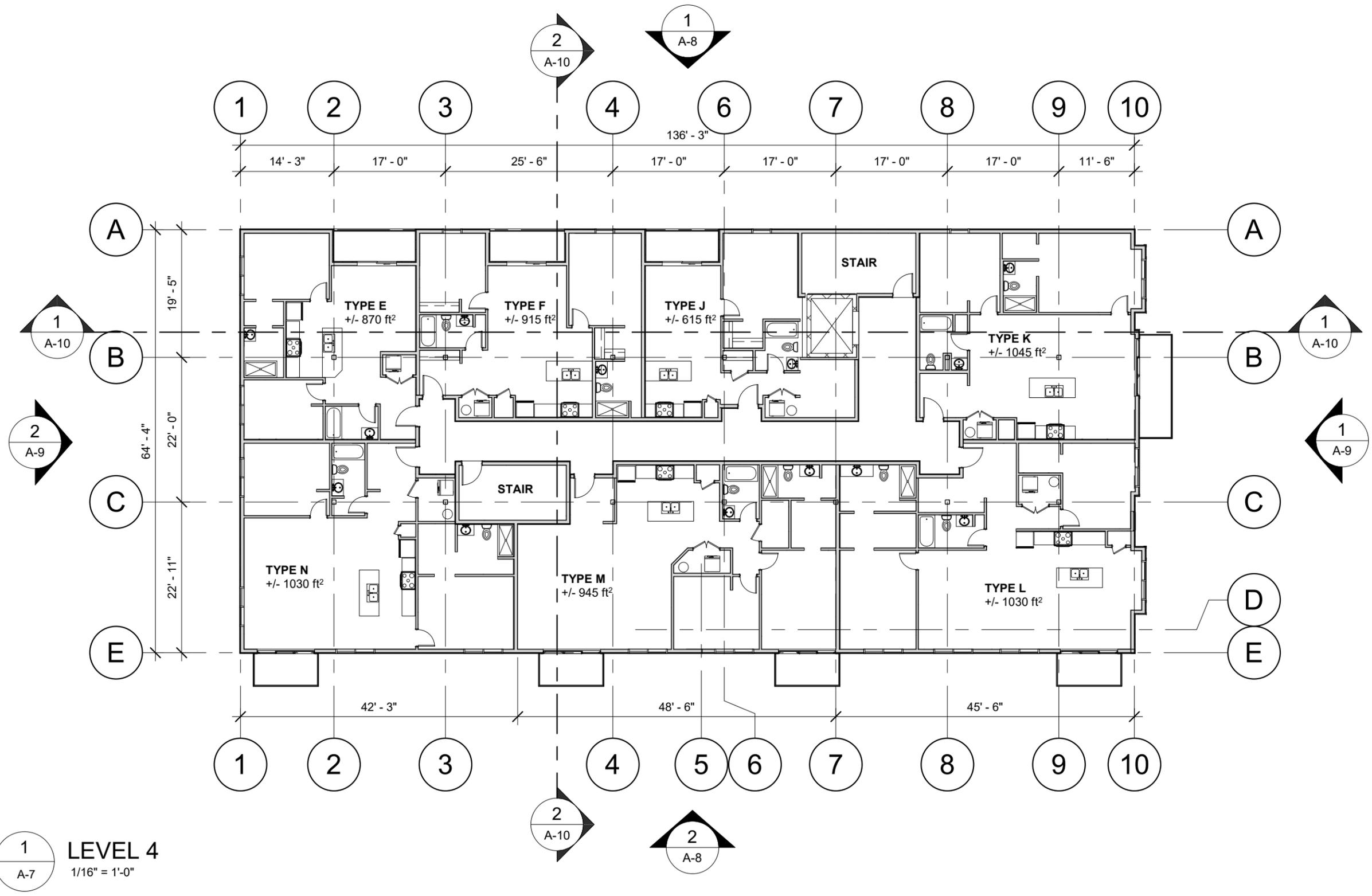
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Client:
Jackie & Phil O'Halloran

Project Title:
**Richmond Street Urban
Housing Infill**

Sheet Title:
Level 3 Floor Plan

Date: 06/15/23
Dwn By: JLA Chk By: SDM
Project Number:
231024
Drawing Number:
A-6



1 LEVEL 4
A-7 1/16" = 1'-0"

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Client:
Jackie & Phil O'Halloran

Project Title:
Richmond Street Urban
Housing Infill

Sheet Title:
Level 4 Floor Plan

Date: 06/15/23
Dwn By: JLA Chk By: SDM
Project Number:
231024
Drawing Number:
A-7

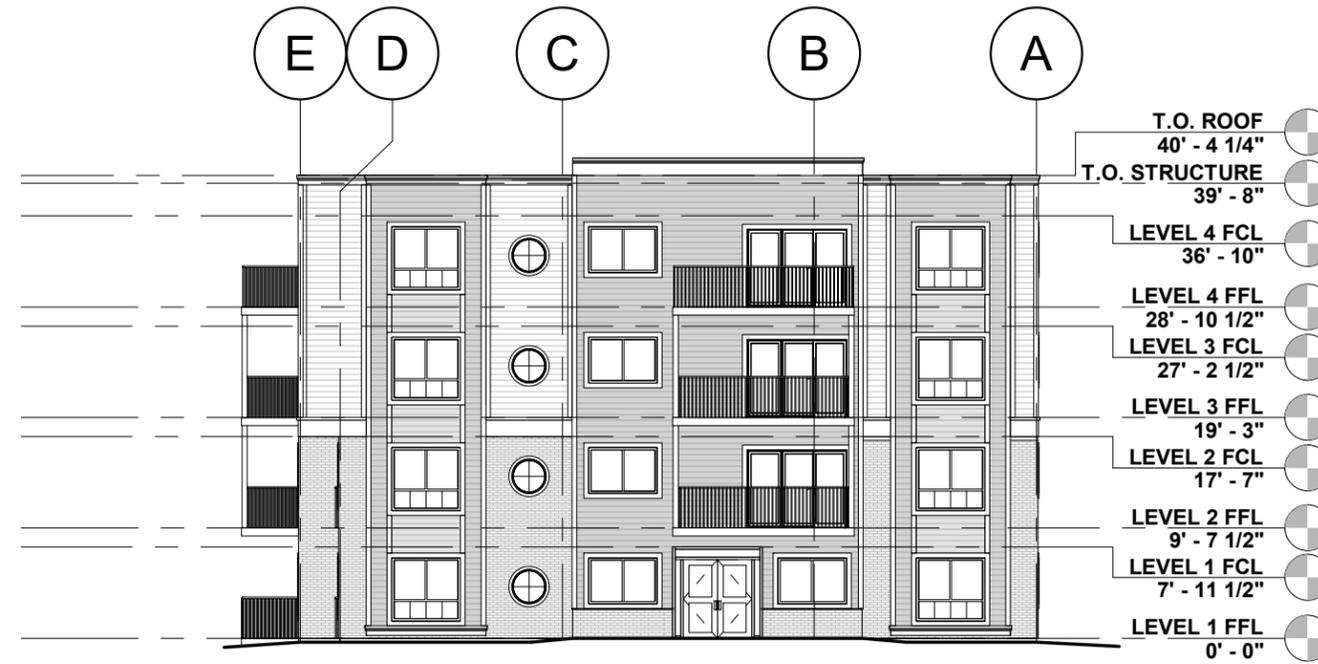


1 EAST ELEVATION
A-8 1/16" = 1'-0"

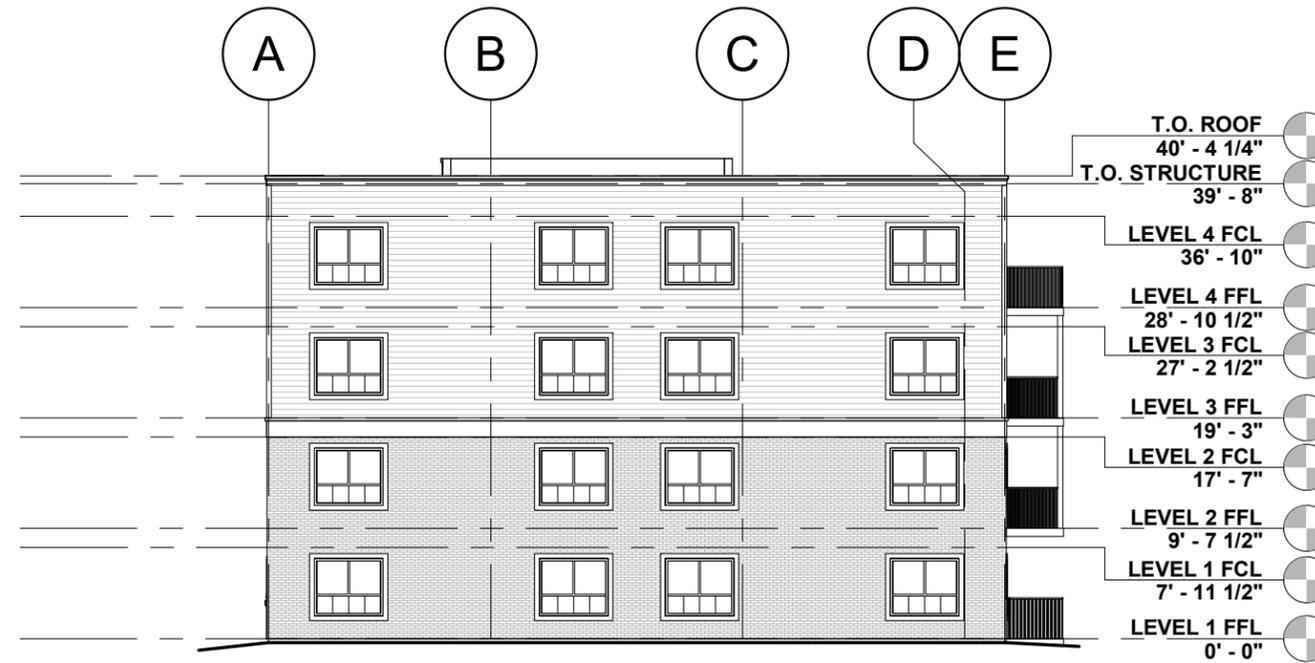


2 WEST ELEVATION
A-8 1/16" = 1'-0"

PRELIMINARY ONLY NOT FOR CONSTRUCTION

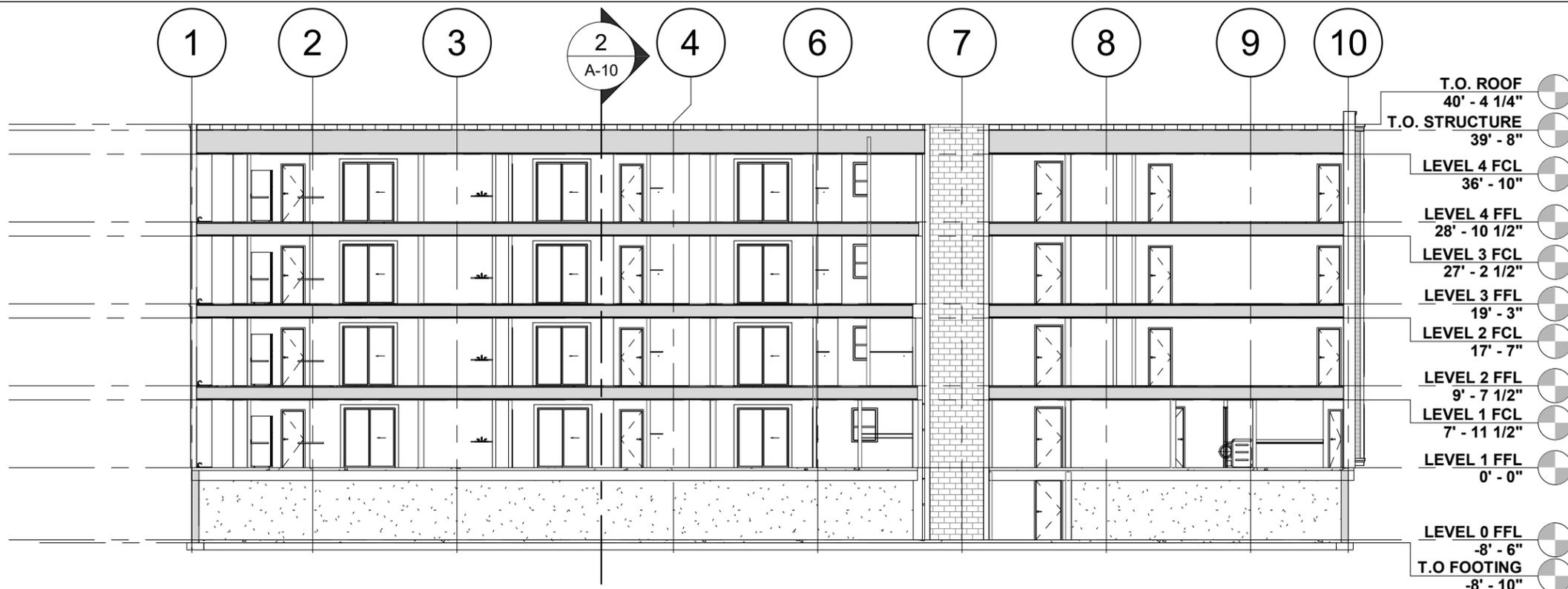


1 SOUTH ELEVATION
A-9 1/16" = 1'-0"



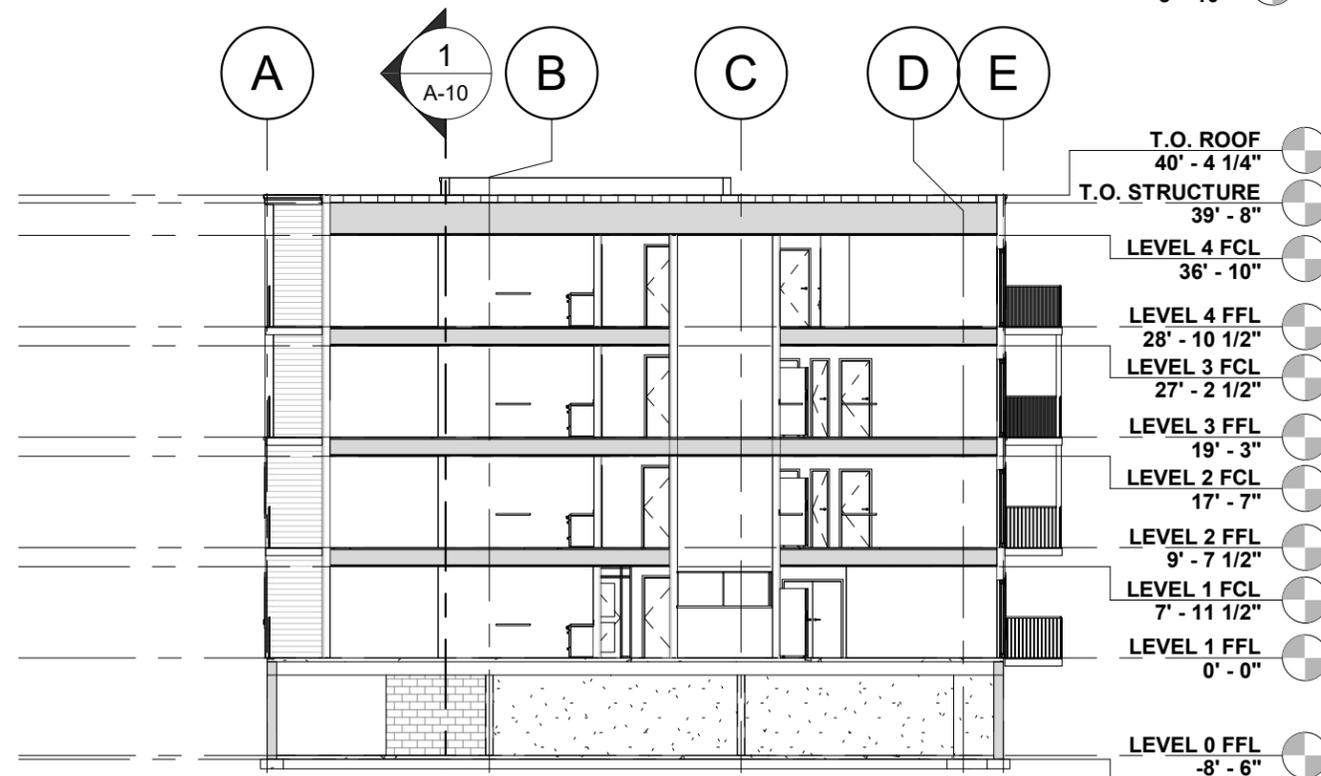
2 NORTH ELEVATION
A-9 1/16" = 1'-0"

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- T.O. ROOF 40' - 4 1/4"
- T.O. STRUCTURE 39' - 8"
- LEVEL 4 FCL 36' - 10"
- LEVEL 4 FFL 28' - 10 1/2"
- LEVEL 3 FCL 27' - 2 1/2"
- LEVEL 3 FFL 19' - 3"
- LEVEL 2 FCL 17' - 7"
- LEVEL 2 FFL 9' - 7 1/2"
- LEVEL 1 FCL 7' - 11 1/2"
- LEVEL 1 FFL 0' - 0"
- LEVEL 0 FFL -8' - 6"
- T.O. FOOTING -8' - 10"

1
A-10
BUILDING SECTION 1
1/16" = 1'-0"



- T.O. ROOF 40' - 4 1/4"
- T.O. STRUCTURE 39' - 8"
- LEVEL 4 FCL 36' - 10"
- LEVEL 4 FFL 28' - 10 1/2"
- LEVEL 3 FCL 27' - 2 1/2"
- LEVEL 3 FFL 19' - 3"
- LEVEL 2 FCL 17' - 7"
- LEVEL 2 FFL 9' - 7 1/2"
- LEVEL 1 FCL 7' - 11 1/2"
- LEVEL 1 FFL 0' - 0"
- LEVEL 0 FFL -8' - 6"
- T.O. FOOTING -8' - 10"

2
A-10
BUILDING SECTION 2
1/16" = 1'-0"

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Client:
Jackie & Phil O'Halloran

Project Title:
**Richmond Street Urban
Housing Infill**

Sheet Title:
Building Sections

Date: 06/15/23
Dwn By: JLA Chk By: SDM

Project Number:
231024

Drawing Number:
A-10



VIEW 1 - PROPOSED 3RD FLOOR EXPANSION AND DECKS



VIEW 2 - PROPOSED 2ND & 3RD FLOOR DECKS WEST ELEVATION

FOR CLIENT REVIEW

PRELIMINARY ONLY NOT FOR CONSTRUCTION

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Client:
Jackie & Phil O'Halloran

Project Title:
Richmond Street Urban
Housing Infill

Sheet Title:
Red House 3D Views, Exterior Concept

Date: 06/15/23

Drn By: JVJ Chk By: SDM

Project Number:
231024
Drawing Number:
A-11

Attachment B

External Design Reviewer Comments (August 8, 2023)

File: DESIGN-2023-30-OCTOBER-6A

19 RICHMOND STREET (PID# 339739)

OWNER: Jackie and Phillip O'Halloran





16 Moore Drive
Murray River PE COA 1WO
902.330.2398
greg@munnarchitecture.ca

August 8th, 2023

Laurel Palmer Thompson
City of Charlottetown Planning and Heritage Department
70 Kent Street, Charlottetown PE
902.629.4108

DESIGN REVIEW of
15-17 Richmond Street, Charlottetown, PE

Dear Laurel,

The following Design Review has been prepared for the proposed development for the 15-17 Richmond Street property, utilizing the design documents received from you on July 28, 2023.

My understanding as a Design Reviewer is that I am to present my observations and recommendations, focused on the affect that a new structure may have on the heritage value of its context. Sections 4 and 5 of the 500 Lot Development Standards & Design Guidelines (December 2011), Heritage Preservation Bylaw (amended March 15, 2019), plus Sections 3.14 DESIGN REVIEW and Section 7 DESIGN STANDARDS FOR THE LOT 500 AREA, of the ZONING AND DEVELOPMENT BYLAW (amended March 28, 2023) have been utilized to guide my observations and recommendations.

For the sake of clarity and clear organization, I have evaluated the designs of the proposed alterations to 15 Richmond separately from the proposed new construction of 17 Richmond. Where appropriate, I have comments on the buildings as a composition.

The proposed project will be sited in the DN (Downtown Neighbourhood) zoned area. 15 Richmond Street, the existing structure within the proposed design, is believed to be the former home of Colonel John Frederick Holland (son of Samuel Holland), who was living in this house as of 1823¹. This means the house is at least 200 years old. It is basically original in its street façade configuration and massing. Its form and stature have been maintained. Unobtrusive modifications have been made over time, such as window pattern, and modifications to the Scottish dormer roof.

I am not aware of the history of the rowhouses to the east of the proposed development but their form suggests they are possibly of a similar vintage as that of 15 Richmond, within a generation or so. Across the street are residential and commercial buildings, likely constructed in the 1970s, and a parking lot. This is the immediate context in which the new construction will be placed. The evaluations and recommendations below are placed in context of, and more focused on the flanking period buildings, and not so much the late 20th century buildings across the street.

1. SITING AND ORIENTATION

¹ Rogers, Irene, Charlottetown The Life in Its Buildings.

15 Richmond: The building is existing and is set back slightly from the street allowing for a small front yard, as do all of the existing building on the north side of the street on this block, with slight variations in depth. In accordance with 7.2 of the Zoning and Development Bylaws, and Section 5.1.1 of the 500 Lot Standards and Guidelines, the house has, and will retain, one primary entrance facing the street.

RECOMMENDATIONS: No recommendations.

17 Richmond: The façade of the proposed building is in line with 15 Richmond, and slightly set back from the rowhouse to the east. The building will have one entry facing the street.

RECOMMENDATIONS: It is my opinion that the siting and orientation of the proposed development meets the 500 Lot Standards and Guidelines, and Section 7 of the Zoning and Development Bylaw. No recommendations.

2. PARKING ACCESS AND GARAGES

The two buildings are in effect considered to share one lot, so my review will cover both properties together. Parking will be at the rear of 15, and access to the parking garage beneath 17 will be accessed from this lot. Access to the parking from the street is from a narrow lane that will be located between 15 and 17. This access lane will be unobtrusive and will not impede on the view or rhythm of the development or street façade.

RECOMMENDATIONS: It is my opinion that the parking access and access to the parking garage of the proposed development meets the 500 Lot Standards and Guidelines, and Section 7 of the Zoning and Development Bylaw. No recommendations.

3. MASS AND SCALE

15 Richmond: The house is existing, its mass and scale are traditional and authentic, and the proposed design does not modify the massing and scale of the house on the street side. The proposed balconies on the west side will be set behind the window bay on the main floor, and relatively far from the street, so the view of and from the street of this added feature will not impede or visually affect the mass and scale of the house. The balconies are reversable and removable in the future without detriment to the house, in keeping with accepted preservation practices, and will not be detrimental to the character of the property.

The proposed shed dormer on the rear of the house is a traditional form and modification of a house of this type, and is appropriate.

RECOMMENDATIONS: I suggest that the balconies and shed dormer are appropriate and acceptable modifications to this house. No recommendations.

17 Richmond: The buildings to the east and west of the proposed building are two storeys with high pitched roofs, and an attic storey expressed by dormers. The gables are on the sides, presenting the shingled roofs to the street. The multi-unit housing across the street is three storeys with a flat roof.

The datum line above the second floor of the proposed building successfully aligns within reason with the soffits and top of wall of the flanking houses, presenting a change in materials that separates the first two floors from the upper two floors. This is a sympathetic architectural gesture and relates the façade to the neighbours. However, this is diluted and obscured by the four-storey vertical protruding

central block element on the front façade of the building that does not carry the datum line. This is the mass that includes the balconies and main entry. Furthermore, the flanking buildings are Colonial, and their character is planar and simple with little interruption in form. The existing houses have a horizontal aesthetic, whereas the proposed new building has a vertical aesthetic. The narrow window bays at the east and west ends of the front façade do not adversely detract from the datum line and are a contemporary interpretation of the traditional window bay and are suitable for the composition.

The proposed building is four storeys, a floor taller than the neighbours, but approximately the same height as the peaks of the flanking houses, and is at the maximum height allowed for a building in the Lot 500 area of 40'. However, the 500 Lot Standards and Guidelines do not allow four-storey buildings in Downtown Neighbourhood (DN) areas. Section 4.0 Development Standards: Downtown Neighbourhood section K. states a maximum of three storeys in this zone.

RECOMMENDATIONS: To address the vertical vs. horizontal issue with the design, it is recommended that the vertical nature of the protruding center mass is broken with the continuation of the datum line across it, or some other solution to downplay the vertical nature of the massing. There are several ways to address this issue and I will leave the design to the architect, but would suggest the vertical/horizontal play on the East side elevation is more successful, as the datum divides the vertical elements (see walls at grid lines 3 and 4, East Elevation). The row house to the east is much wider than the proposed new building, and therefore, widening the planes of the proposed building would make it blend in better with its neighbours.

Although the height of the building is within the 40' height limit, the proposed building is four-storey tall. As noted, this is not allowed in the DN zones of Charlottetown. It is recommended that the building design is revised and presented again as a three-storey building.

4. ROOFS

15 Richmond: The roof of the proposed shed dormer is a traditional and common form and appropriate for this house. The roof finish is not shown in the proposal material.

RECOMMENDATIONS: I recommend that the roof is finished in shingles to match the existing roof of the house. If the roofing of the house is to be replaced, the shingles of the dormer are to match those of the new shingles. Either asphalt shingles or cedar shingles will be acceptable.

17 Richmond: The roof of the new building will be flat, a traditional roof within the Lot 500 zone.

RECOMMENDATIONS: No Recommendations.

5. PORCHES AND ENTRIES

15 Richmond: No change to the existing front enclosed porch/ main entry of the house is proposed. Railings of the wooden steps and deck leading to the front door are missing from the conceptual design.

RECOMMENDATIONS: Add or repair the railings to the front door. Ensure the height for the handrail is within allowances of the NBCC. Railings are to be constructed of painted wood and of a character that compliments the house.

17 Richmond: The main entry of the building is through double glazed doors, roughly centered on the façade of the building. According to the site plan, the level difference between the sidewalk and the

main floor is .308m, which is just shy of 12". The exact length of the walk is not noted but it is approximately 12'. It is noted that the walk will have a slope of 12:1. If so, it is legally a ramp and will require handrails. This configuration does not allow for a level landing at the top of the ramp, which is required to be a minimum of 5' deep to allow for seating, see 7.8.1 of the Zoning and Development Bylaws, and Section 5.1.9 of the Lot Standards and Guidelines,

RECOMMENDATIONS:

Design a level landing area at the front doors at least 5' deep, and include a ramp with rails no more than 12:1 or a sloped walkway great that 1:20 if handrails are not desirable. If any steps are included in the design, include handrails, even though the height does not require them according to NBCC.

6. WINDOWS

15 Richmond: The window configuration is double one-over-one sashes, in keeping with existing windows on the house on either side. The finish materials have not been noted for the windows.

RECOMMENDATIONS: The windows are recommended to be painted wood sashes and trim, not metal clad, metal or vinyl.

17 Richmond: As required by 7.5.1 of the Zoning and Development Bylaw, and Section 5.1.6 of the Lot Standards and Guidelines, the windows are of consistent style and configuration on all sides of the building, except, appropriately, where the building is in close proximity to the building to the immediate east. The patterning of the windows are balanced and consistent. There is a discrepancy between the 3D concept images and the south elevation. The 3D imagery shows a French door with two large windows to one side, whereas the elevations show glass sliding doors. The muntin patters are not traditional, but this is acceptable as it helps to define the new building as Contemporary.

Although the windows are of a contemporary configuration (fixed picture with operable sliders at bottom, or large casements) the shape, rhythm and opacity is relatively consistent with those of the neighboring buildings.

RECOMMENDATIONS: According to 7.5.4 of the Zoning and Development Bylaws, sliding doors are not permitted on the front façade of a building within the 500 Lot area. Therefore, these will not be acceptable as presented in the South Elevation, but what is portrayed in the 3D imagery is permitted. Please clarify that the French door option will be installed.

7. MATERIALS AND FINISHES

In the opinion of this reviewer, the Charlottetown Lot 500 zone is traditionally a wood, brick and stone City. The use of these materials is encouraged. However, metal is a contemporary building material, and is appropriate in certain instances to define a building as contemporary, as contrasting with quality materials is acceptable as to not confuse historic/ period structures with new.

15 Richmond: The existing material finish of the house is wood clapboard siding, with narrow wood trim, painted. The materiality of the structure, decking and finish of the balconies has not been noted. The railings appear to be glass. The finish materials have not been noted for the clapboard siding or trim of the shed dormer.

RECOMMENDATION: I recommend that the finish of the structure of the balconies and decking is painted wood to match the materiality of the house. The clapboard siding and trim of the shed dormer is recommended to be painted wood, and the width of the clapboard siding (the “to weather” dimension) is to match that of the rest of the house.

17 Richmond: The cornice and trim at the datum lineyuo has a contemporary Classical Revival aesthetic, which is appropriate for this building, as it sits between two Colonial Classical Revival buildings, and carried the detail aesthetic of these houses. The darker first two floors help to blend the scale of the new building with the existing on either side. The Weathered Cedar cladding is problematic as 7.4.5 of the Zoning and Development Bylaw states that “The appearance of building materials shall be true to their nature and should not mimic other materials.”.

The metal cladding is an acceptable finish material in the 500 Lot area, as this product is not addressed either as permitted or not permitted. It provides scale and pattern that compliments the scale of clapboard siding and brick, so is true to its use in the 21st Century.

Brick is a traditional material in the Lot 500 zone, and therefore is acceptable.

As noted in **3. MASS AND SCALE**, the neighbouring houses are Classical Revival, and retain their original aesthetic with minor modifications over time, but still true to their nature. Typically, the materials utilized on a building of this period are limited to one and sometimes two materials.

RECOMMENDATIONS: It is recommended that the metal Weathered Cedar Cladding is not used on the building.

To compliment the period houses flanking the new construction, it is recommended that the palette is simplified. The other three sides of the building are actually more in keeping with the aesthetic of the other buildings on the street as the palette is simpler and the mass of the building is less sub-divided.

8. LANDSCAPING

15 Richmond: The existing three trees in the front yard of the house are being retained. There is an existing dry laid Island sandstone retaining wall east of the front porch to the driveway that is not represented in the concept imagery. The Allan Block retaining wall is an appropriate use of materials as it is a contemporary material for contemporary use.

RECOMMENDATIONS: Retain or salvage for reuse the stones to keep or rebuild the retaining wall in its original location, as far as possible to tie into the new retaining walls of the new driveway. Colour of the Allan Block retaining wall has not been specified. Warm colours would be preferable to raw concrete grey, to blend better with the landscaping.

17 Richmond: Section 7.9.1 states that a landscaping area shall be provided between the sidewalk and the front façade of the building, where appropriate for a continuous streetscape. This is provided in the Site Plan of the proposal, with the suggestion of trees planted in the yard, which is appropriate, acceptable and desirable. Other areas of the site not occupied by the buildings or parking and the lane are assumed to be grass lawn.

RECOMMENDATIONS: No recommendations.

SUMMARY

15 Richmond: I recommend the acceptance of the design modifications to 15 Richmond Street. These are minor and sympathetic modifications to a real survivor, a grand house that has seen 2 centuries of Island history, and continues to evolve be remain relevant and a contributing character in the story of Charlottetown.

17 Richmond Street: The design The number of floors proposed for the new building is a violation of the Lot 500 regulations for a DN zone. It is my opinion that the fourth floor on the street face will adversely affect the scale by visually standing above the other buildings on the street, interrupting its rhythm and scale. Although the building is within the height allowed, a 40' vertical wall will look like it stands twice as tall as the existing 20' walls flanking it, and may create a canyon between it and the 30' wall across from it. The issue of massing and scale is more than height and length. It's also, how much sky can you see when you are standing in front of it. Therefore I cannot recommend building to be built as designed, and that the design should be reconsidered as to how it meets the street.

Sincerely,

A handwritten signature in blue ink, appearing to read "Greg Munn". The signature is fluid and cursive, with a long horizontal stroke at the end.

Greg Munn, Architect,
President, Munn Architecture Inc.
AAPEI President, MRAIC, ROAC PEI Delegate, APT PEI Delegate

Attachment C

Revised Design Reviewer Submission (September 12, 2023)

File: DESIGN-2023-30-OCTOBER-6A

19 RICHMOND STREET (PID# 339739)

OWNER: Jackie and Phillip O'Halloran





VIEW ALONG RICHMOND STREET 1



VIEW ALONG RICHMOND STREET 2

PRELIMINARY ONLY NOT FOR CONSTRUCTION

CORNICE AND BATTENS



METAL CLADDING - 1



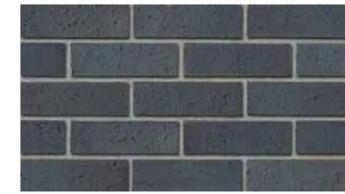
Regent Grey
56082
(22ga)

METAL CLADDING - 2

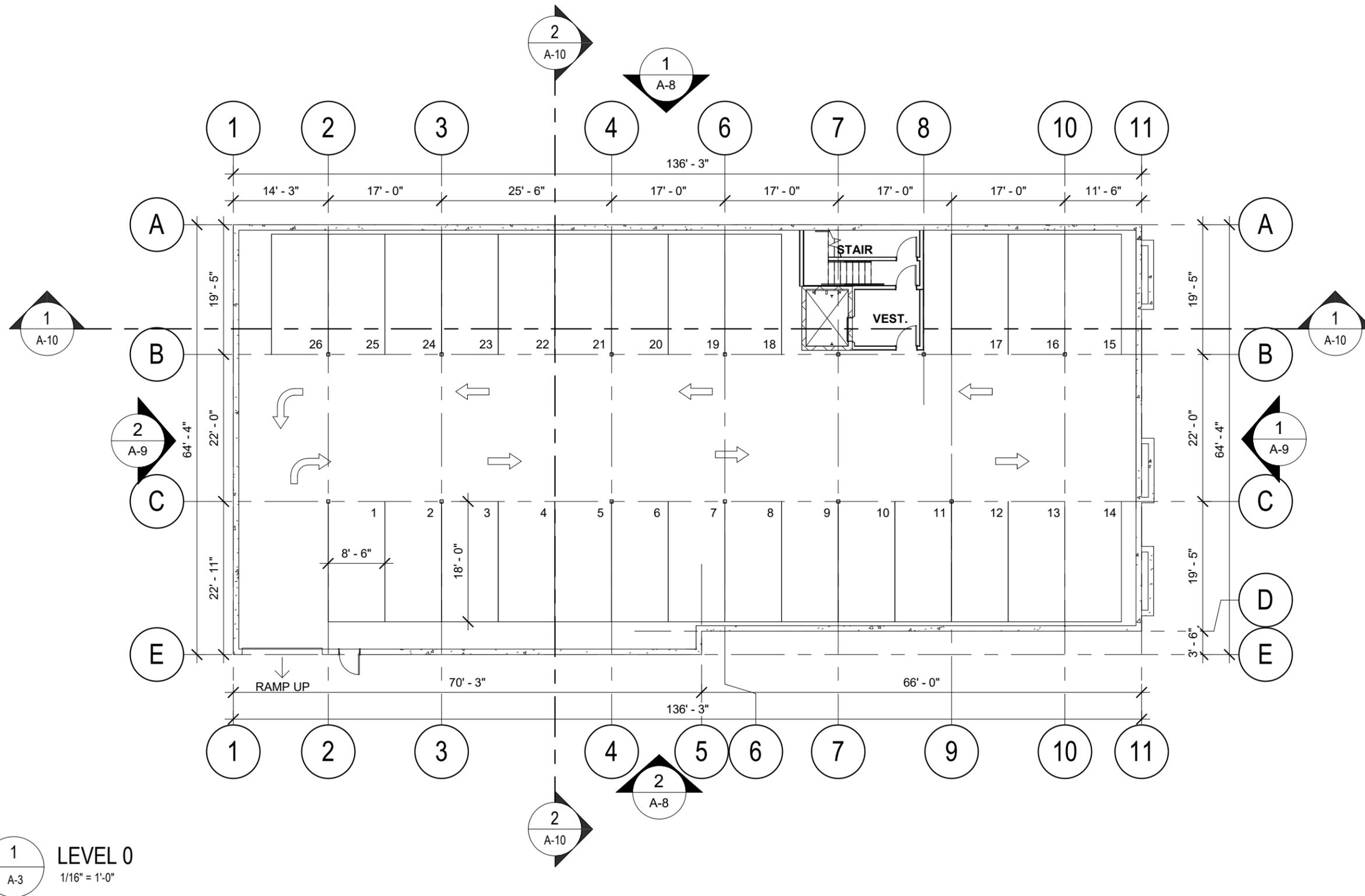


Stone Grey
56071
(22ga)

BRICK



PRELIMINARY ONLY NOT FOR CONSTRUCTION



1 LEVEL 0
A-3 1/16" = 1'-0"

PRELIMINARY ONLY NOT FOR CONSTRUCTION

COLES
Architecture + Engineering + Project Management

Suite 201, 85 Fitzroy Street
Charlottetown, P.E.I., Canada, C1A 1R6
Phone: (902) 368-2300
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Client:
Jackie & Phil O'Halloran

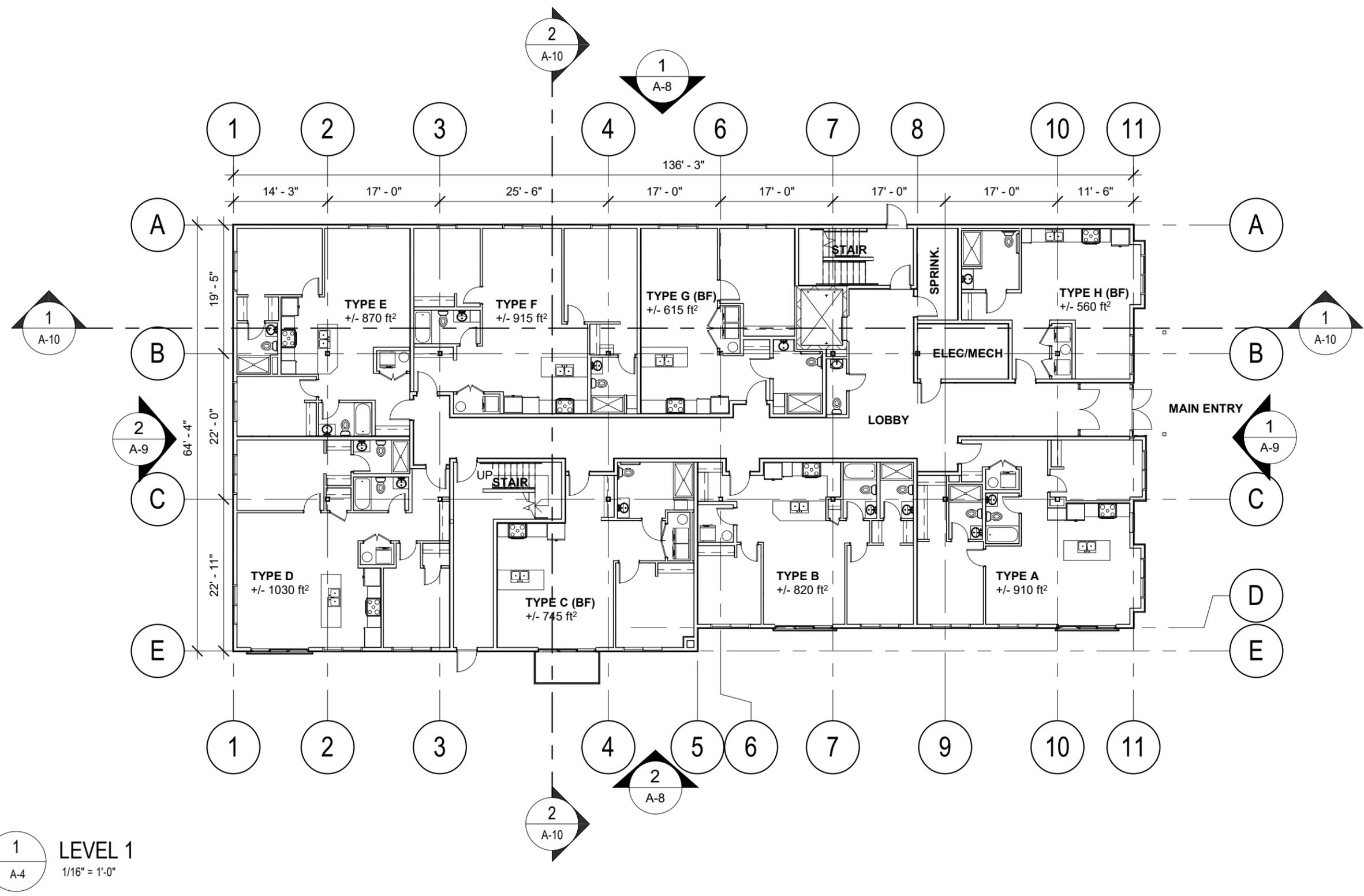
Project Title:
Richmond Street Urban
Housing Infill

Sheet Title:
Level 0 Floor Plan

Date: 09/12/23

Drn By: JLA Chk By: SDM

Project Number:
231024
Drawing Number:
A-3



1 LEVEL 1
A-4 1/16" = 1'-0"

PRELIMINARY ONLY NOT FOR CONSTRUCTION

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Architecture + Engineering + Project Management

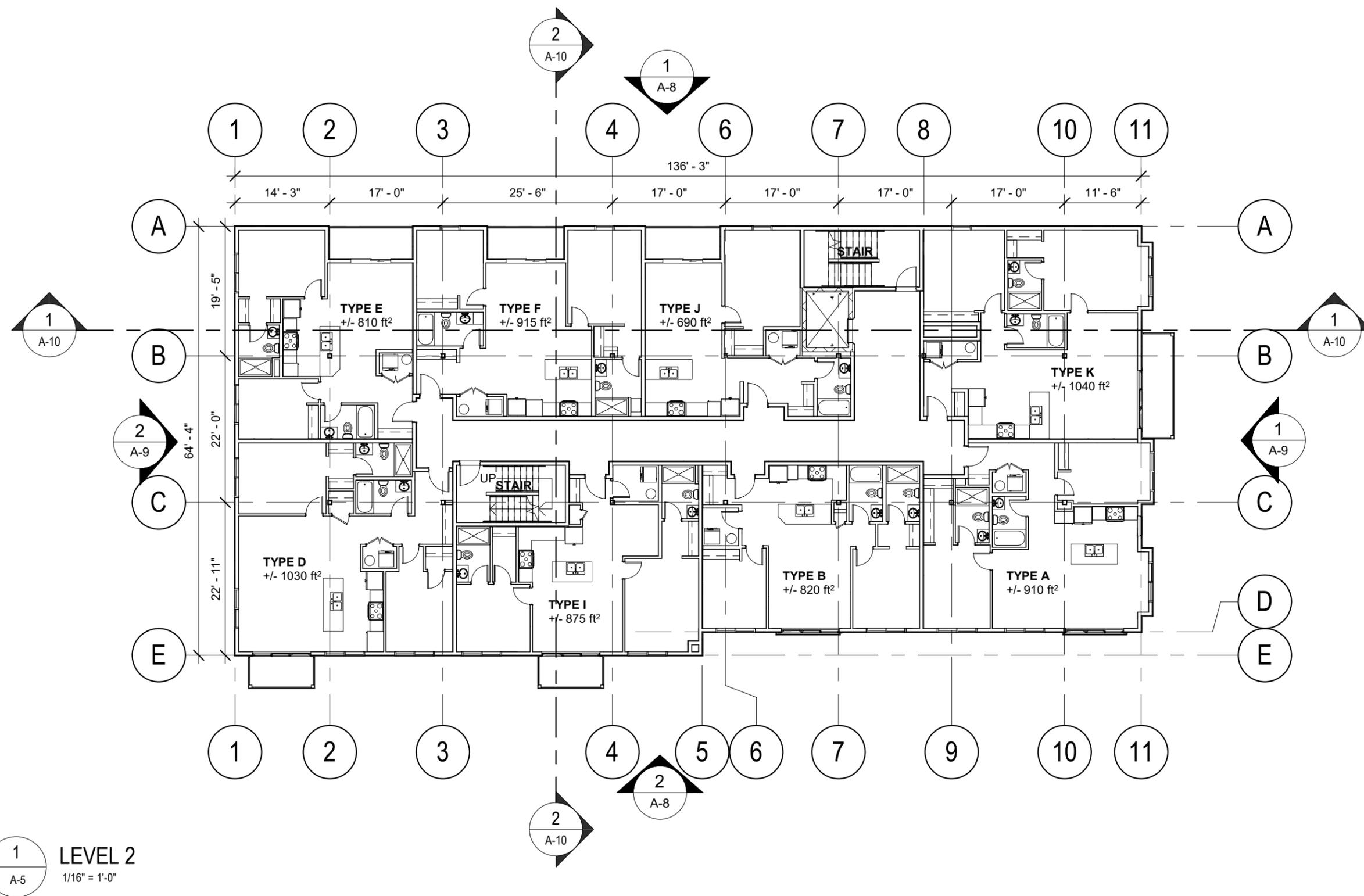
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Charlottetown, P.E.I., Canada, C1A 1R6
Phone: (902) 368-2300
www.colesassociates.com

Client:
Jackie & Phil O'Halloran

Project Title:
**Richmond Street Urban
Housing Infill**

Sheet Title:
Level 1 Floor Plan

Date: 09/12/23
 Drn By: JLA Chk By: SDM
 Project Number:
231024
 Drawing Number:
A-4



1
A-5
LEVEL 2
1/16" = 1'-0"

PRELIMINARY ONLY NOT FOR CONSTRUCTION



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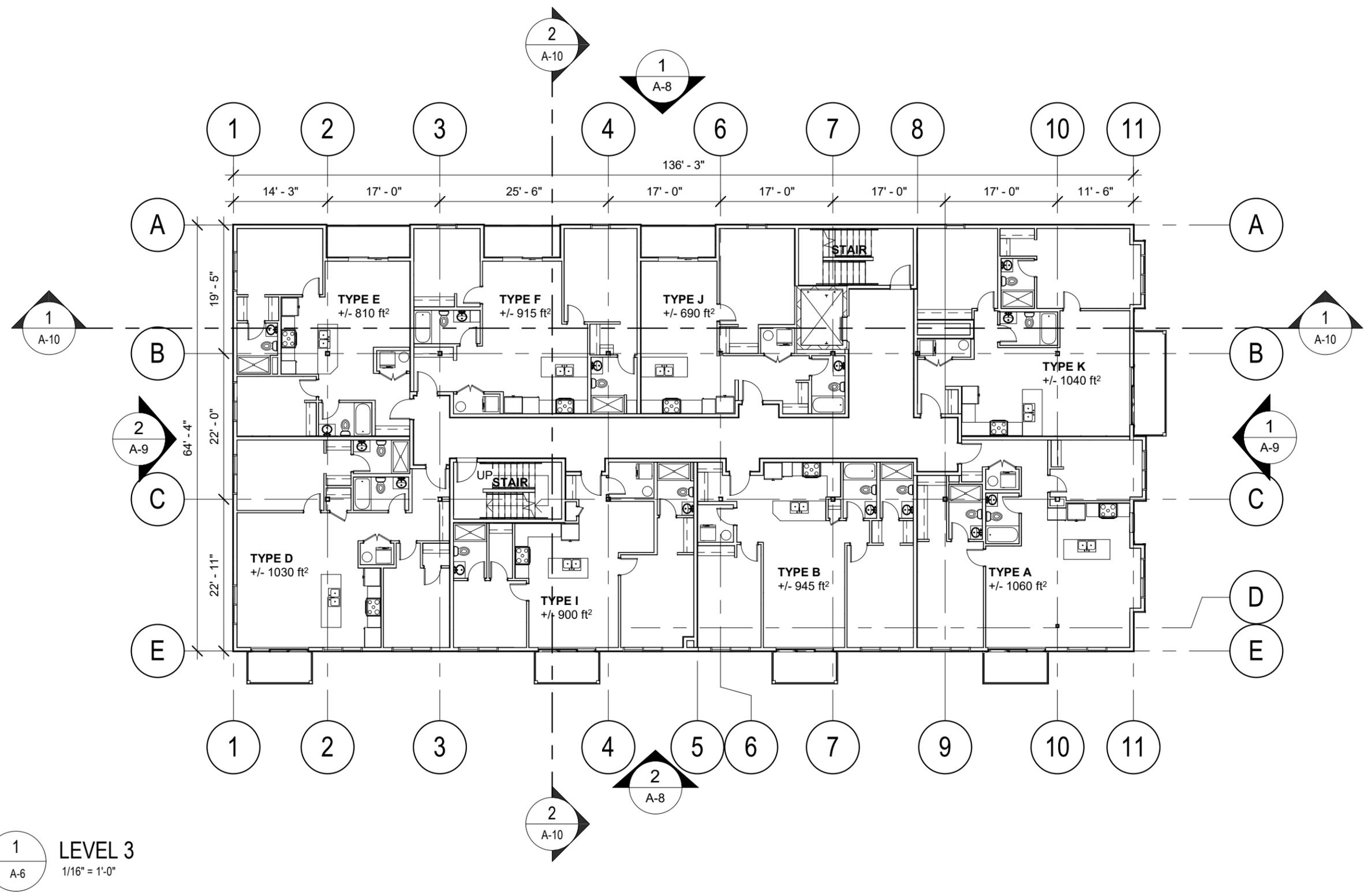
Client:
Jackie & Phil O'Halloran

Project Title:
**Richmond Street Urban
Housing Infill**

Sheet Title:
Level 2 Floor Plan

Date: 09/12/23
Dwn By: JLA Chk By: SDM

Project Number:
231024
Drawing Number:
A-5



1 LEVEL 3
A-6 1/16" = 1'-0"

PRELIMINARY ONLY NOT FOR CONSTRUCTION

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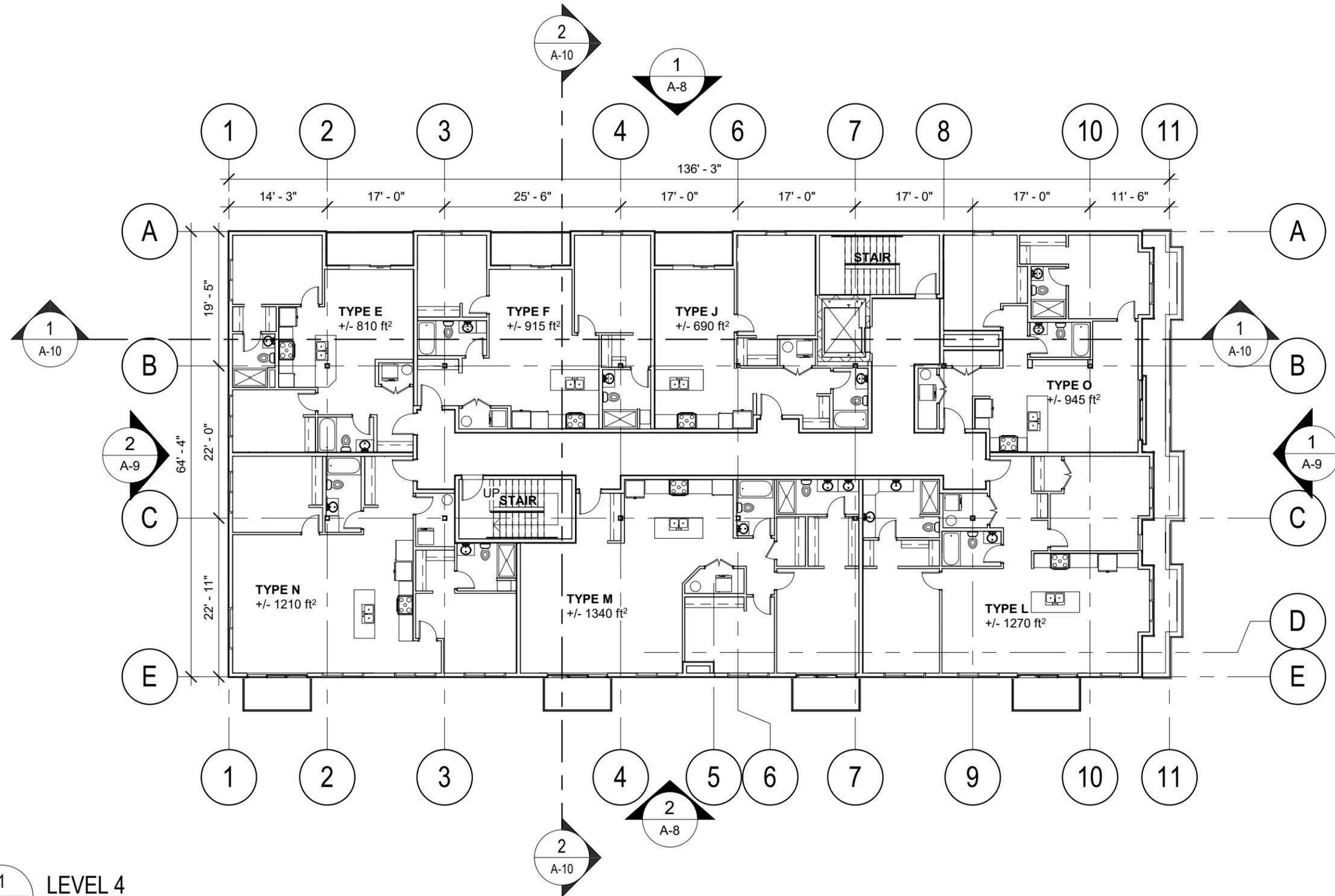
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Charlottetown, P.E.I., Canada, C1A 1R6
Phone: (902) 368-2300
www.colesassociates.com

Client:
Jackie & Phil O'Halloran

Project Title:
**Richmond Street Urban
Housing Infill**

Sheet Title:
Level 3 Floor Plan

Date: 09/12/23
 Dwn By: JLA Chk By: SDM
 Project Number:
231024
 Drawing Number:
A-6



1 LEVEL 4
A-7 1/16" = 1'-0"

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Charlottetown, P.E.I., Canada, C1A 1R6
Phone: (902) 368-2300
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Client:
Jackie & Phil O'Halloran

Project Title:
Richmond Street Urban
Housing Infill

Sheet Title:
Level 4 Floor Plan

Date: 09/12/23

Drn By: JLA Chk By: SDM

Project Number:
231024
Drawing Number:
A-7



1
A-8
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1/16" = 1'-0"

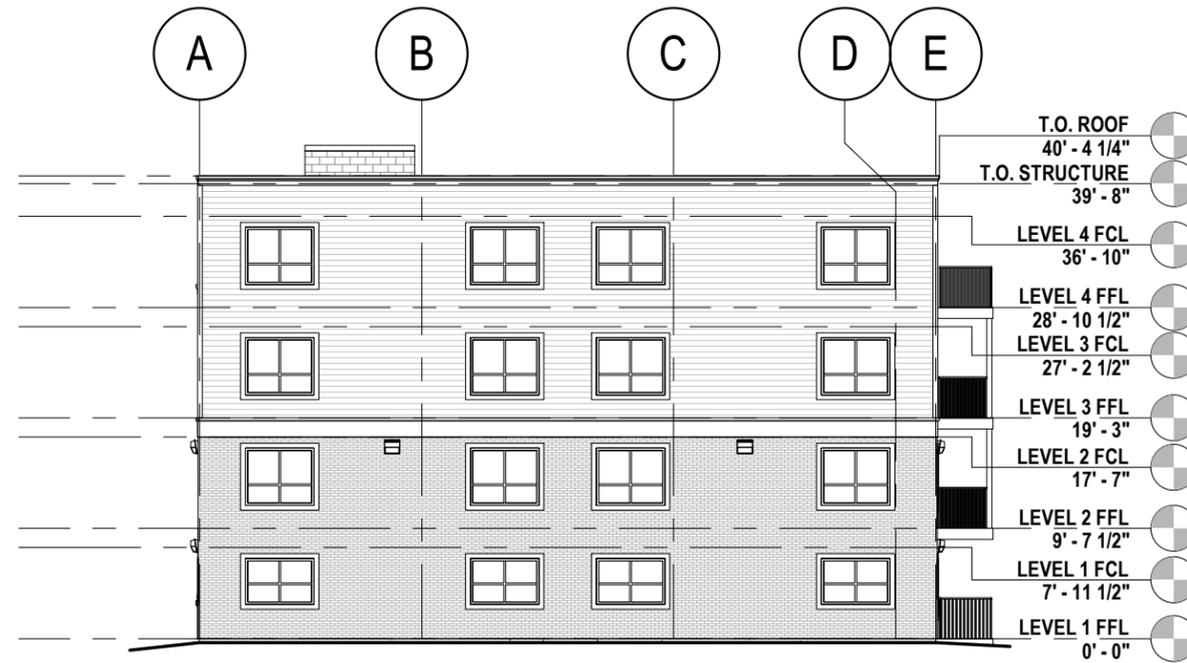


2
A-8
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1/16" = 1'-0"

PRELIMINARY ONLY NOT FOR CONSTRUCTION

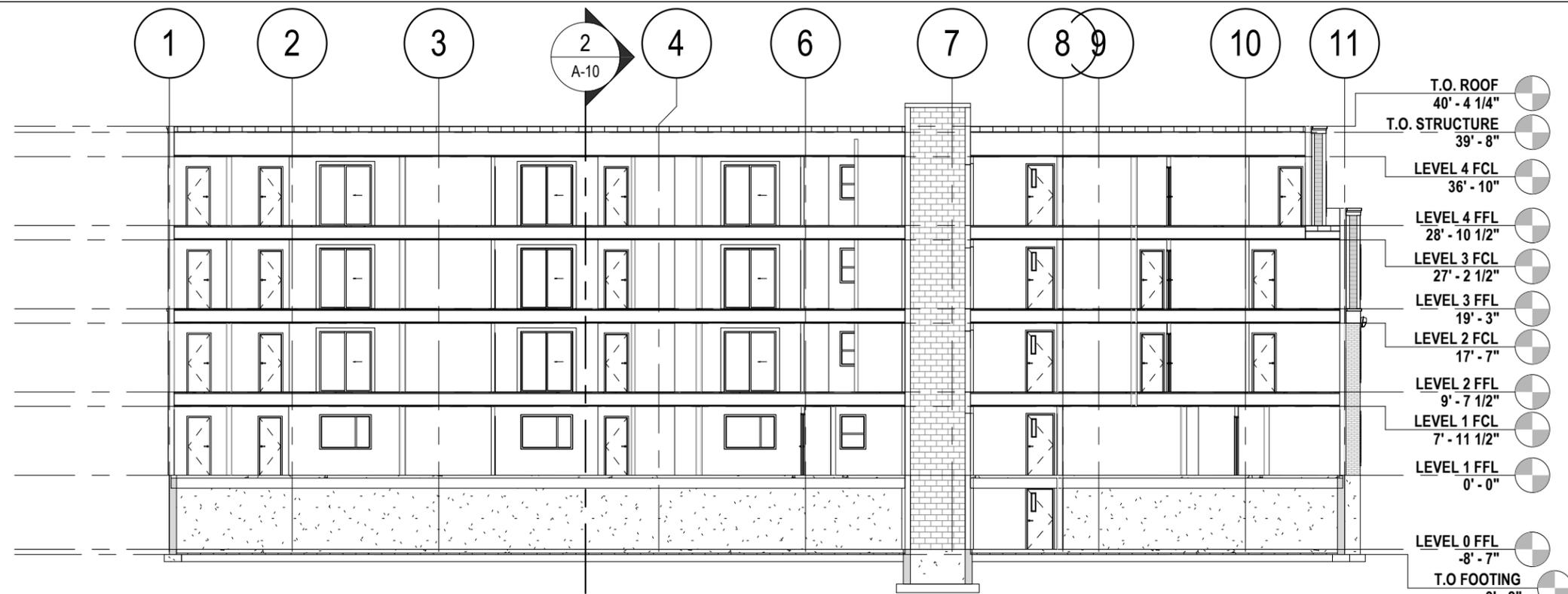


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A-9
DR SOUTH ELEVATION
1/16" = 1'-0"

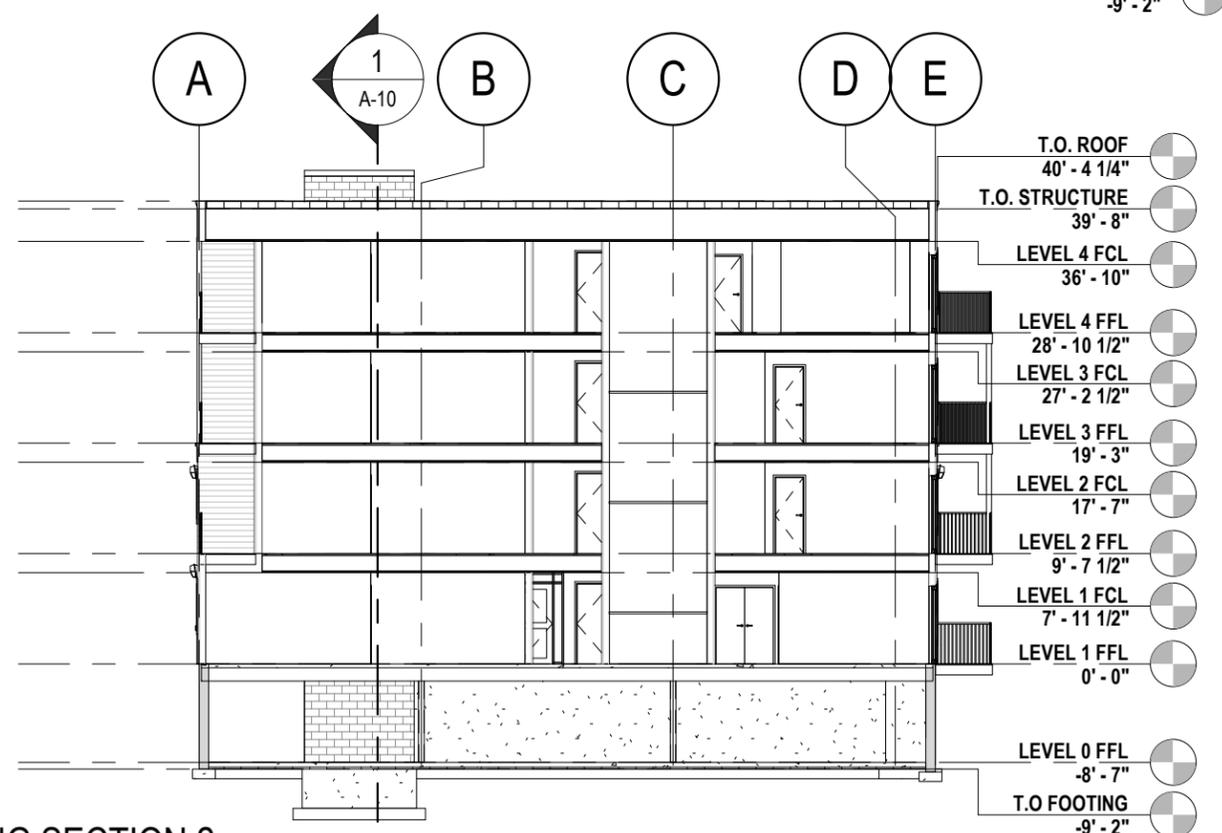


2
A-9
DR NORTH ELEVATION
1/16" = 1'-0"

PRELIMINARY ONLY NOT FOR CONSTRUCTION



1
A-10
DR BUILDING SECTION 1
1/16" = 1'-0"



2
A-10
DR BUILDING SECTION 2
1/16" = 1'-0"

PRELIMINARY ONLY NOT FOR CONSTRUCTION

Attachment D

Final Design Submission (October 24, 2023)

File: DESIGN-2023-30-OCTOBER-6A
19 RICHMOND STREET (PID# 339739)

OWNER: Jackie and Phillip O'Halloran





VIEW ALONG RICHMOND STREET 1



VIEW ALONG RICHMOND STREET 2

PRELIMINARY ONLY NOT FOR CONSTRUCTION

CORNICE AND BATTENS



METAL CLADDING - 1



Regent Grey
56082
(22ga)

METAL CLADDING - 2

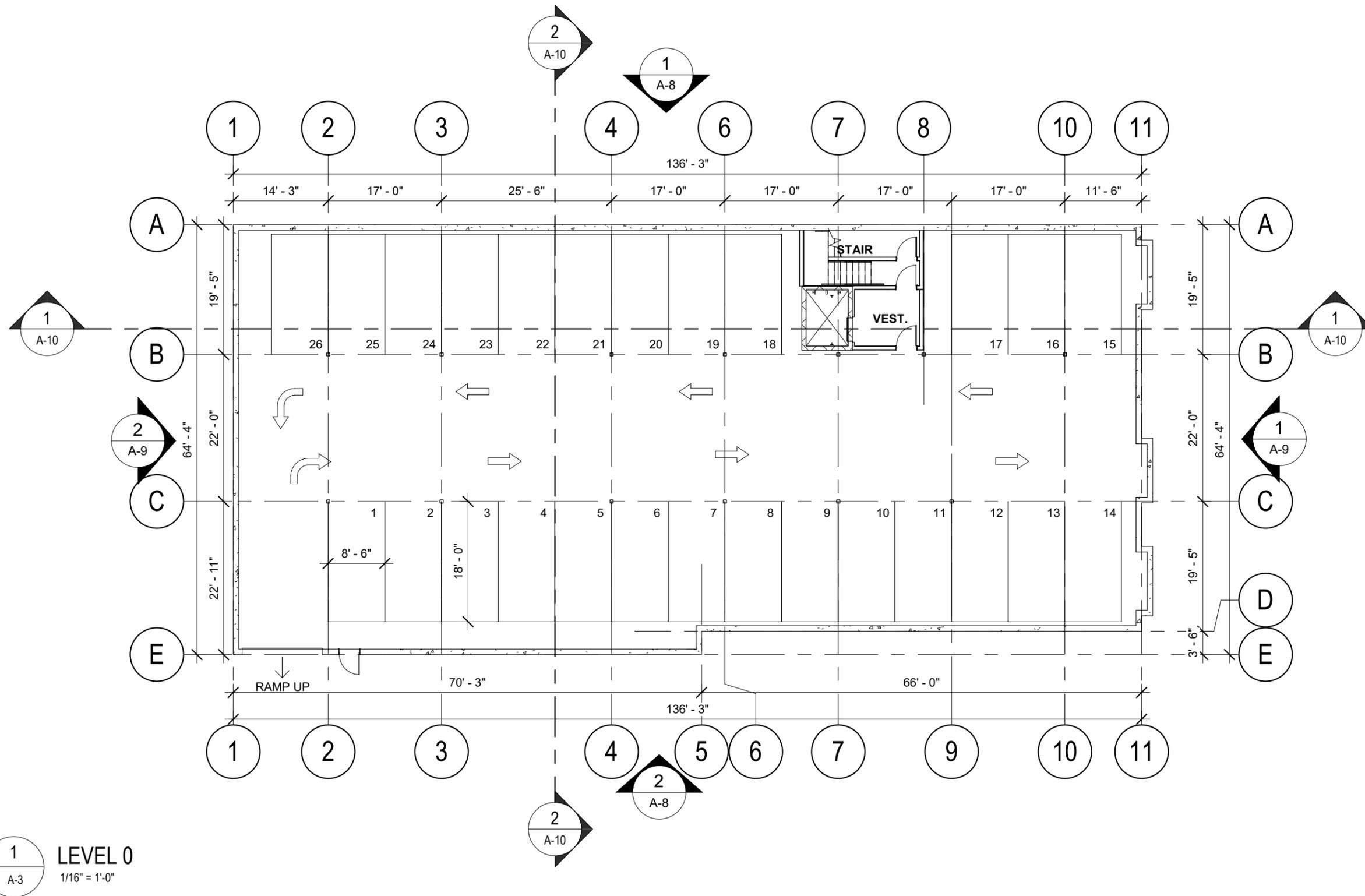


Stone Grey
56071
(22ga)

BRICK



PRELIMINARY ONLY NOT FOR CONSTRUCTION



PRELIMINARY ONLY NOT FOR CONSTRUCTION

1 LEVEL 0
A-3 1/16" = 1'-0"



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Client:
Jackie & Phil O'Halloran

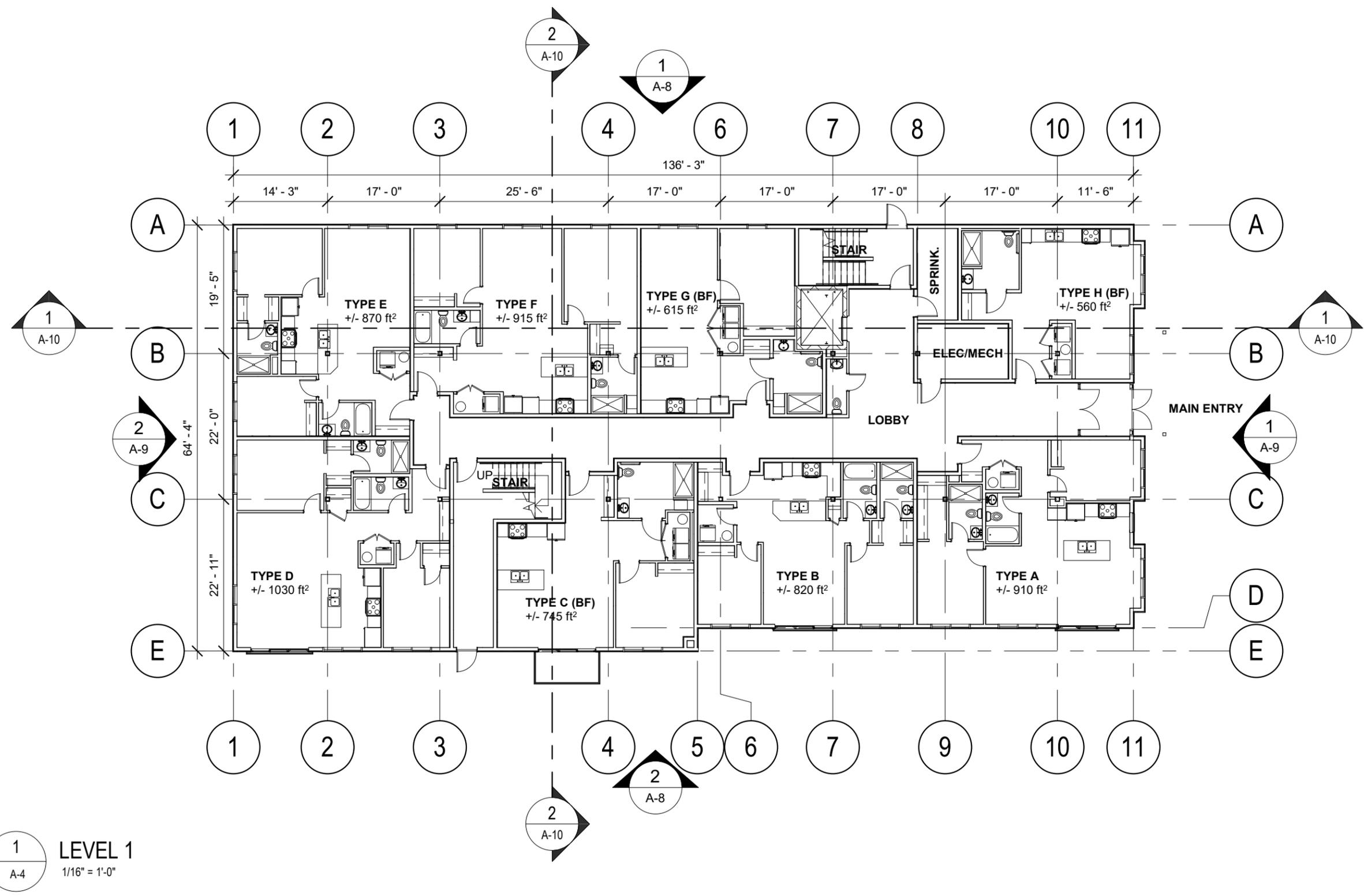
Project Title:
Richmond Street Urban
Housing Infill

Sheet Title:
Level 0 Floor Plan

Date: 10/24/23

Drn By: JLA Chk By: SDM

Project Number:
231024
Drawing Number:
A-3



1
A-4

LEVEL 1
1/16" = 1'-0"

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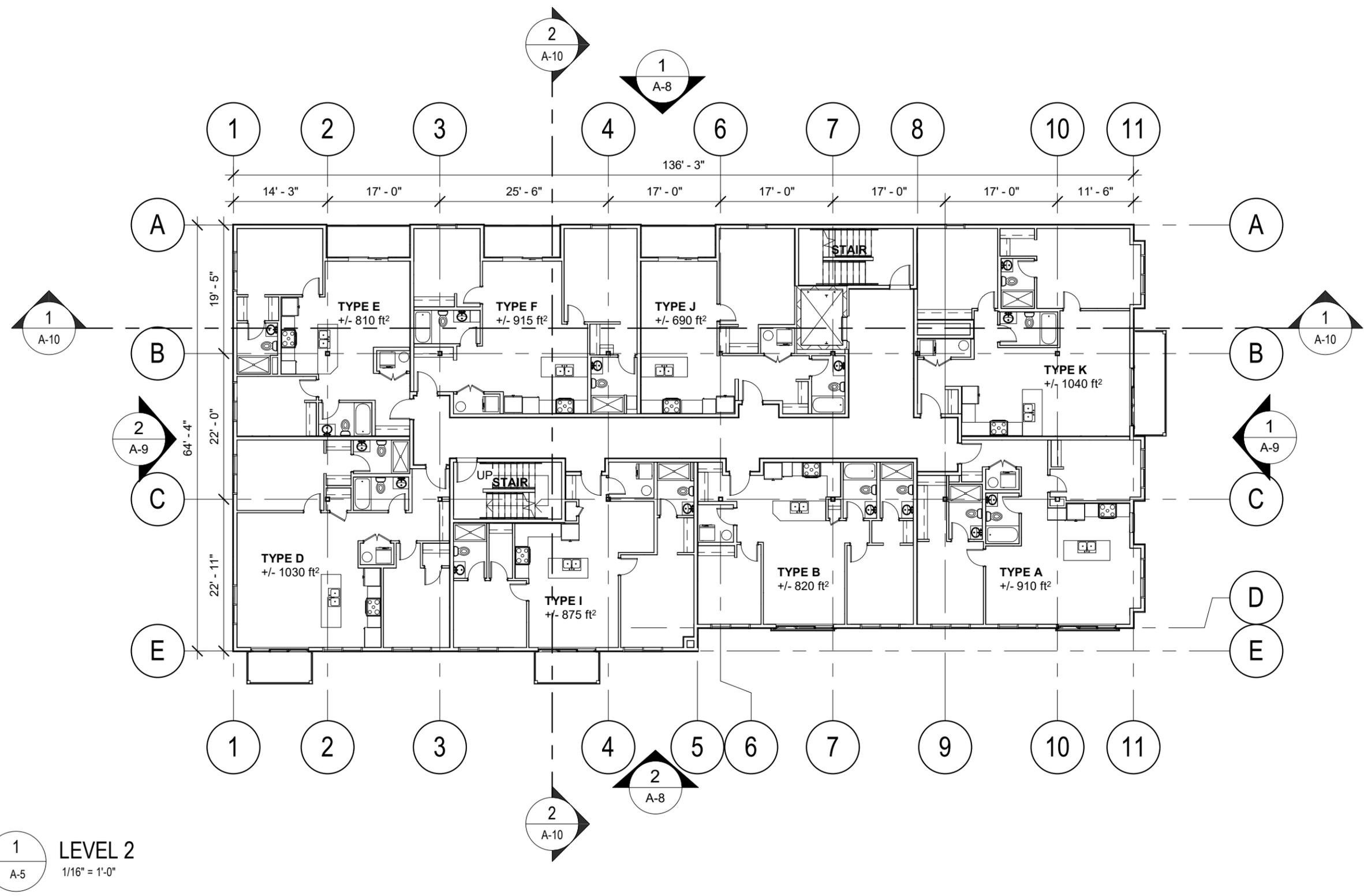
Client:
Jackie & Phil O'Halloran

Project Title:
Richmond Street Urban
Housing Infill

Sheet Title:
Level 1 Floor Plan

Date: 10/24/23
Dwn By: JLA Chk By: SDM

Project Number:
231024
Drawing Number:
A-4



1
A-5

LEVEL 2
1/16" = 1'-0"

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Phone: (902) 368-2300
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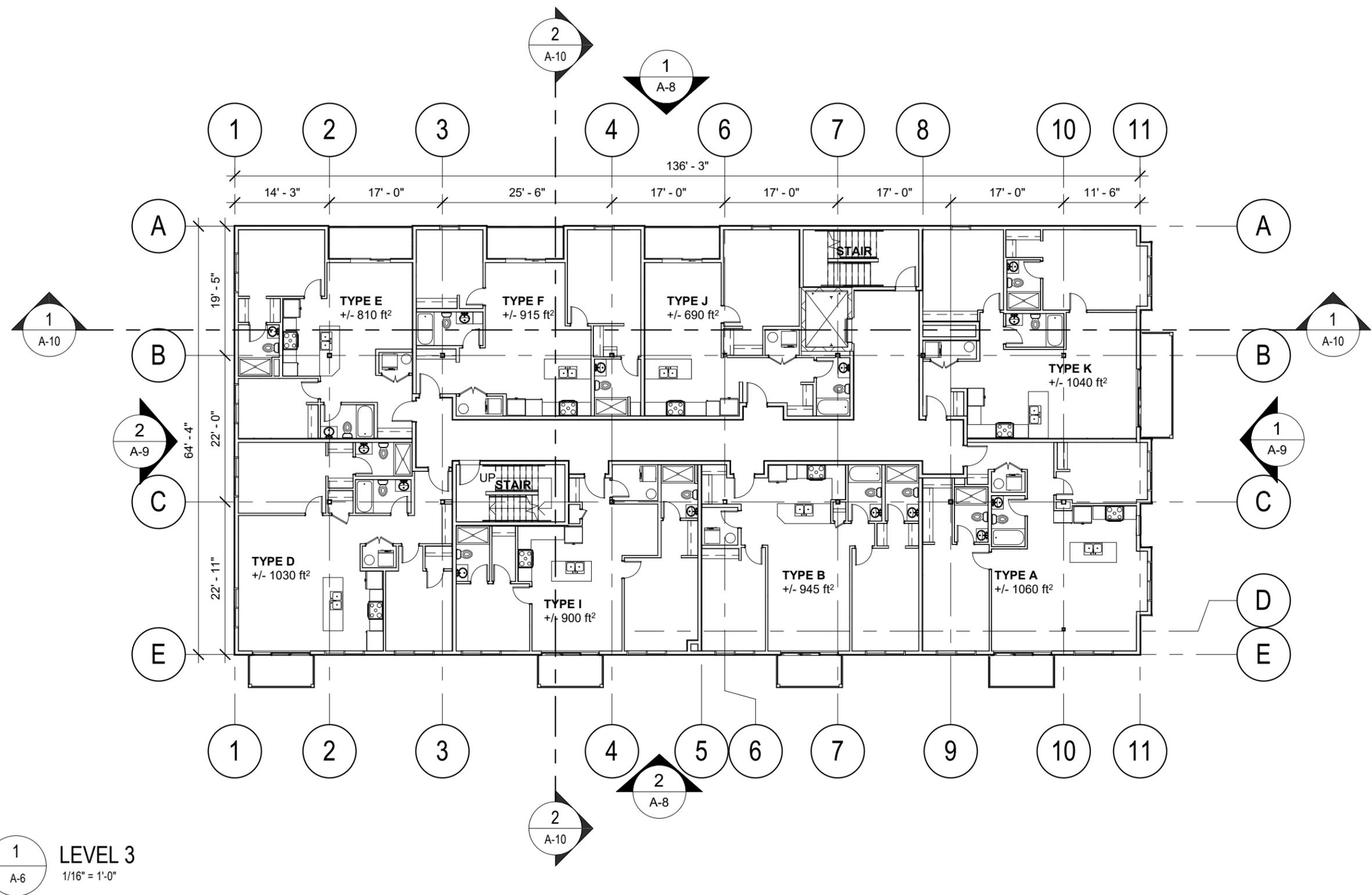
Client:
Jackie & Phil O'Halloran

Project Title:
**Richmond Street Urban
Housing Infill**

Sheet Title:
Level 2 Floor Plan

Date: 10/24/23
Dwn By: JLA Chk By: SDM

Project Number:
231024
Drawing Number:
A-5



1 LEVEL 3
A-6 1/16" = 1'-0"

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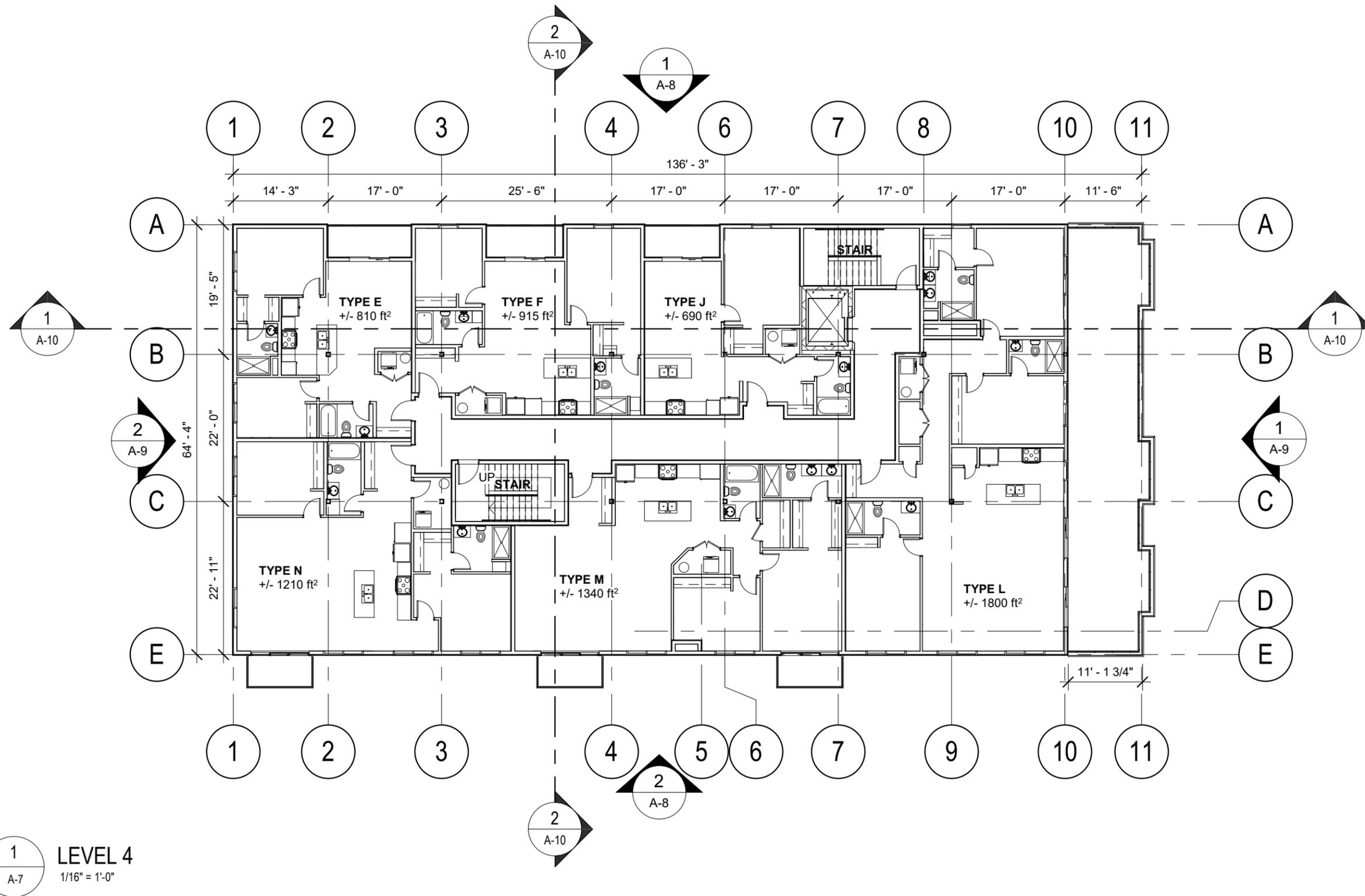
Client:
Jackie & Phil O'Halloran

Project Title:
Richmond Street Urban
Housing Infill

Sheet Title:
Level 3 Floor Plan

Date: 10/24/23
Dwn By: JLA Chk By: SDM

Project Number:
231024
Drawing Number:
A-6



1 LEVEL 4
A-7 1/16" = 1'-0"

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Phone: (902) 368-2300
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Client:
Jackie & Phil O'Halloran

Project Title:
Richmond Street Urban
Housing Infill

Sheet Title:
Level 4 Floor Plan

Date: 10/24/23

Drn By: JLA Chk By: SDM

Project Number:
231024
Drawing Number:
A-7

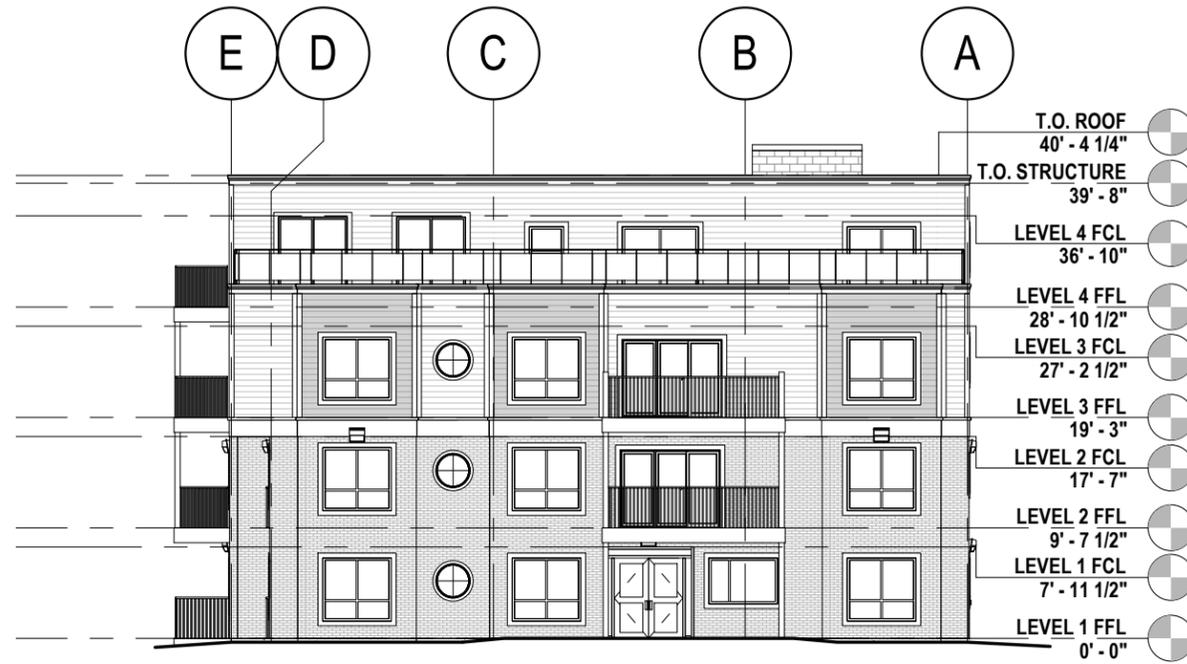


1
A-8
DR EAST ELEVATION
1/16" = 1'-0"

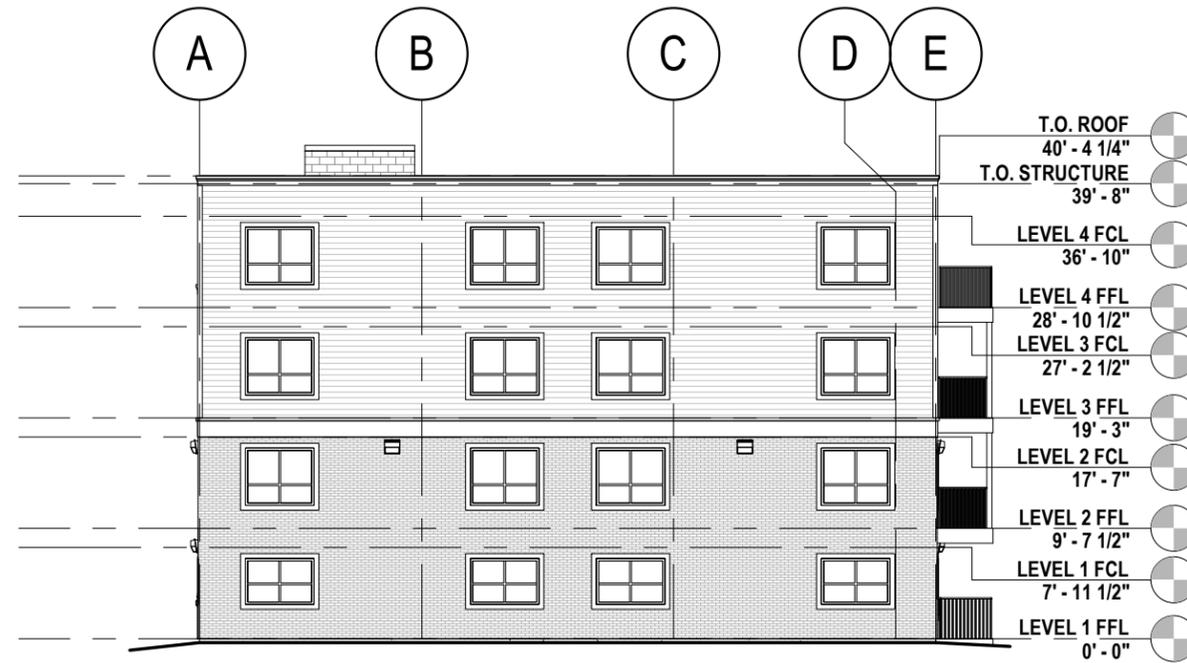


2
A-8
DR WEST ELEVATION
1/16" = 1'-0"

PRELIMINARY ONLY NOT FOR CONSTRUCTION

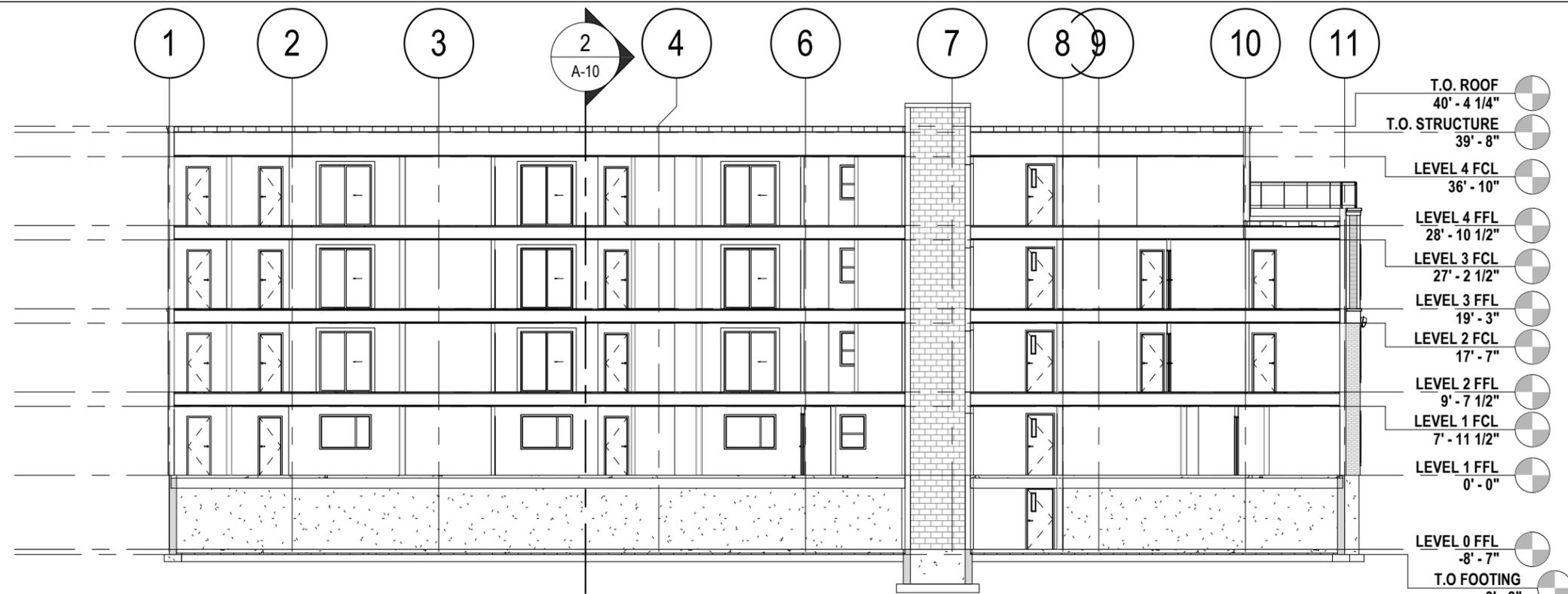


1
A-9
DR SOUTH ELEVATION
1/16" = 1'-0"

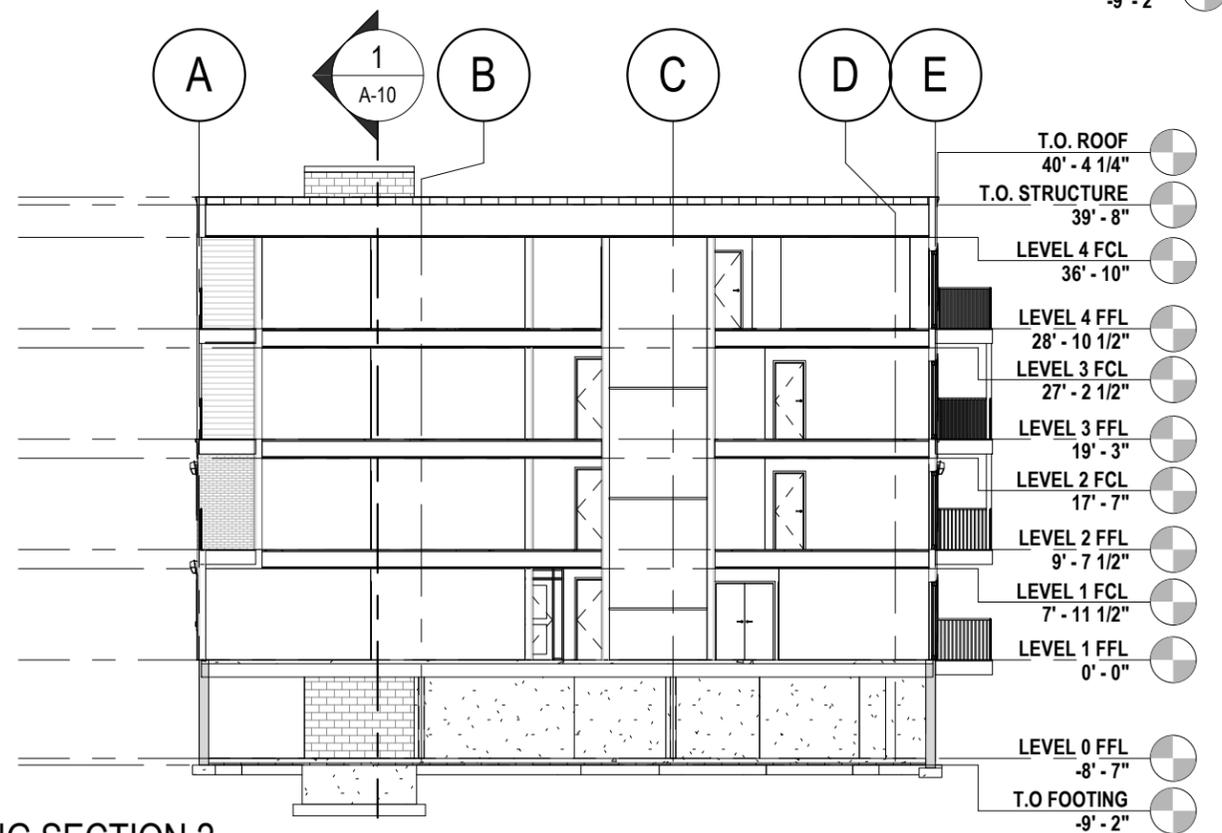


2
A-9
DR NORTH ELEVATION
1/16" = 1'-0"

PRELIMINARY ONLY NOT FOR CONSTRUCTION



1
A-10
DR BUILDING SECTION 1
1/16" = 1'-0"



2
A-10
DR BUILDING SECTION 2
1/16" = 1'-0"

PRELIMINARY ONLY NOT FOR CONSTRUCTION

TITLE: DESIGN REVIEW FILE: DESIGN-2023-30-OCTOBER-6B 199 GRAFTON STREET/156 PRINCE STREET (PID #342790) OWNER: 102390 PEI INC. APPLICANT: APM COMMERCIAL ROYAL LEPAGE		 CHARLOTTETOWN	
MEETING DATE: October 30, 2023		Page 1 of 7	
DEPARTMENT: Planning & Heritage		ATTACHMENTS: A. Design Review Submission Including Rationale Paper (August 25, 2023) B. External Design Reviewer Comments (October 26, 2023) C. Design Review Submission (February 21, 2021) D. Map showing Location Of Property	
SITE INFORMATION: Ward No: 1 - Queens Square Existing Land Use: Commercial / office building and surface parking lot Official Plan: Downtown Mixed Use Neighbourhood Zoning: Downtown Mixed Use Neighbourhood Zone (DMUN)			

RECOMMENDATION:

The Planning & Heritage Department encourages the Design Review Board to approve the proposed building façade plans for the proposed eight (8) storey, one hundred fifty-eight (158) unit apartment building with parking located within and under the building located at 199 Grafton Street (PID #342790) as per the Design Reviewer’s recommendations. Furthermore, the Design Review Board’s Recommendation will be forwarded to the Planning Board outlining their assessment of the variances and design.

BACKGROUND:

Request

APM Commercial on behalf of their client 102390 PEI Inc. has submitted a revised design for a proposed eight (8) storey, multi-residential building consisting of 6 storeys of apartments on top of a 2 storey parking garage at grade and one level of underground parking. The proposal includes a variance to exceed Bonus Height. This application was before the Board in March of 2021 and received Design Review Approval and also received approval from Council through a site-specific amendment to construct the building to 6 stories with bonus height and with a variance for an additional 10 feet above bonus height. The applicant has indicated in their submission that the

current proposal is to incorporate 32 affordable housing units and 126 market units for a total unit count of 158 units.

The applicant has stated in their project rationale, (see below)

It is our stance that providing such elements as distinct building character, additional green space, access to more affordable housing in the downtown core, a generous setback from Prince Street with open green space, sustainable building features, adequate parking and direct access to community services (via Grafton Place), should be more than enough incentive and public benefit to mitigate any concerns relating to the variances we require.

Having identified no significant commonality or distinction surrounding the area, the design for the proposed building adapts to what we feel is appropriate for this site with distinct proportions through clearly defined vertical and horizontal forms that break up the scale of the building. Each volume is then characterized with a different material or color and finally recessed balconies, along with exaggerated cornices further enhance and animate the building along Prince Street where it is most important.

If the current design proposal is approved by the Design Review Board the applicant will be required to proceed to Planning Board for a recommendation on their request for a site-specific amendment to construct the building to have 8 storeys. The site-specific amendment would also include a variance to the step-back requirement on Prince Street.

These components of the application will be deferred to Planning Board and Planning Committee. The current request is to approve the revised design proposal.

By-law Requirement

As per Section 7.1 of the Zoning and Development By-law, new buildings within the 500 Lot Area are required to undergo the Design Review process. This is to ensure that the architectural design of proposed development within the 500 Lot Area maintains a higher quality of design and is constructed with a consistent type and quality of materials. From a design perspective, “is a building of its time” which compliments and is compatible with existing buildings on surrounding properties. Please refer to the Design Standards of the 500 Lot Area Section 7 of the Zoning and Development Bylaw (attached). The standards outlined in this section are the requirements under review as a part of this application.

ANALYSIS:

The applicant submitted a written and graphic submission outlining their revised proposal to construct an 8 storey building. The submission included façade drawings, floor plans and a site plan of the proposed multi-unit building (see Attachment A). The proposed building design submission was sent to an independent design reviewer, Greg Munn, a licensed Architect, to perform a design analysis.

On October 26, 2023, Mr. Munn submitted his formal review, for further information please refer to Attachment B, which indicated the following main points:

PARKING ACCESS AND GARAGES

RECOMMENDATIONS: The brick faced, three-storey mass of the Prince Street façade of the proposed building contains traditionally proportioned vertical window openings. These openings are placed in a well composed and dynamic pattern of solid and void spaces, that sometimes do align vertically, and sometimes don't, as they reflect the configuration of the spaces within. It is a successful contemporary interpretation of a traditional form. It is recommended that the Hillsborough face of the parking garage carries the same aesthetic of openings and voids, with vertical openings that match the size and shape of the Prince Street windows, without glass, composed across the façade, instead of the wide openings. It is recommended that the metal panels are removed from the design and the material is limited to brick. The goal and intent of the Bylaw is to make the garage facing the street seem more for people than cars, and this would tie the two ends of the building together architecturally, as well introduce a human scale to the façade that will better relate to the neighbouring houses.

MASS AND SCALE

RECOMMENDATIONS: Two options to consider: 1. is recommended that the top two floors of the 8-storey proposal are stepped back to achieve the 45-degree angular plane bylaw. If this option is not desirable, then a variance will be required to override the Bonus Height Development Standards regarding the required 45-degree angular plane setback. 2. Extend the east end of the building closer to Hillsborough Street, to compensate for the units lost if the building were to stay six storeys, see image top of next page. The shaded area would potentially provide an equal number of units without going higher than six storeys.

WINDOWS

RECOMMENDATIONS: Option 1: The small operable awning/ over larger picture unit windows should be replaced with either full height casement windows, with cross muntin true divided lights, or no muntin bars at all. Option 2: The stacked awning panes are to be of equal size to be more in keeping with the aesthetic of the traditional window stock of Charlottetown. Double or single hung windows are not recommended as this will detract from the contemporary aesthetic of the building, but the cross divided lights or equally proportioned awnings will be arguably more modern than the proposed windows, and more appropriate to enhance the character of Charlottetown and its historic context.

SUMMARY

The proposed development will provide much needed housing, and I am encouraged by increasing the number of affordable units from 12 in the initial submission, to 32 proposed in this revised design. However, I must stress that the purpose of this review is to also consider the long view of the impact of this development on the city. Every new building and development sets precedence and must be thoughtfully considered as to its impact on the character and context of the city. The unprecedented building boom seen in Charlottetown over recent years has generally been positive for the city but also means that review and consideration of new developments must be all the more diligent to ensure the City will retain its unique character, built over the past 2 ½ centuries, to remain attractive for growth and prosperity for future generations.

In the design review report the design reviewer indicates that if the 7th and 8th floors are not stepped back to meet the 45-degree angular plane a variance will be required. The design reviewer's report was forwarded to the applicant for comment on the suggested changes and the applicant indicated that,

“While we are encouraged to work with the City on moving forward with this quality development, we still need to be practical and cognizant of costs and feel that this project “as submitted” does meet the parameters of any quality urban infill project.

With that being said, we are willing to compromise as such:

- *Hillsborough Street Parkade elevation...We have no issues with his recommendation and are willing to make the necessary changes.*
- *Window Style...We have no issues with his recommendations in principle, however factory installed mutton bars in today's applications, appear cheap and do not properly represent what you would traditionally find around Charlottetown. We are willing to consider a more balanced window but want to avoid the use of cheap factory installed mutton bars and stick to a clean and more modern look.*
- *Prince Street elevation step backs...We feel this is an unnecessary hurdle with needless structural challenges and added costs, that also disrupts the current location of our elevators and diminishes the size and therefore rent of the upper premiere units along Prince Street. It's for these financial reasons, his recommendation to step the building back is something we are unable to compromise on.*

Extending the building back any further than we have it now is not an option, as we are trying to keep as much of the 5-storey east wing of the polyclinic unobstructed to allow as much light into the offices spaces as possible. This would also negatively impact some of the properties along Kent Steet that back onto Clarke.

As always, we are committed to quality development and positive change within the limits of today's current economic outlook."

A variance to the 45-degree angular plane as well as the variance above bonus height will be required under a site-specific amendment in order to obtain building permit approval if the design is approved as submitted. The variances covered under the 2021 site specific amendment are still in place for the other building components.

Should the Board approve the proposed design without modifications staff will forward a detailed report to Planning Board following public notification of the requested variances.

The request for a Bonus Height Application was approved to 6 storeys in 2021 by the City's Planning Committee.

As per Section 3.12.10, "The City at it's discretion shall provide a recommendation from Planning Committee to the CAO for the Public Benefits that should be required for the development pursuing a bonus height application."

The applicant proposed 12 affordable housing units in the 2021 application but are now proposing 32 affordable housing units and 126 market units for a total of 158 units.

The Design Reviewer has indicated that a step-back on Prince Street is required to meet Section 30.3 c iii. of the Zoning and Development Bylaw which states,

“Applications for a Bonus Height are subject to the following regulations:

A 45-degree angular plane originating from the top of the flank or rear façade of the base Building that faces abutting residential dwellings or within a Downtown Neighbourhood (DN) Zone”.

Staff would request that the Design Review Board pay particular attention to this street frontage to determine whether they concur with the Design Reviewer’s assessment. Prince Street is a significant streetscape. If the Design Review Board deems the step-back not to be significant the current design will be included as a variance under the request for a site-specific amendment.

When this application goes to public hearing staff and Council will hear from the adjoining property owners as well as the residents to the north who back onto Clarke Street. As this application has already gone through the public process and was approved to 6 storeys, some concerns were raised by residents for the 6 storey proposal. Staff cannot predetermine what the outcome of the public hearing process will be but would suggest to the Design Review Board that it would be helpful to indicate if they feel the proposed building can or cannot be constructed generally as presented.

If the Design Review Board has concerns regarding the requested height and step-back variances then these concerns should be identified during the design review process. This will ensure that the application proceeds with some level of success to Planning Board as opposed to being returned to the Design Review Board because the variances cannot be supported.

The intent of the Design Review Board is to assess not only the design elements of this project but factor in the requested height variance to determine if design changes can mitigate the impact of the proposed height and possibly a step-back variance. Staff would welcome any suggestions that the Design Review Board would offer regarding the step-back treatment on Prince Street. The Design Review Board needs to be mindful of the requested variances but not necessarily constrained if they feel the final product can be integrated into this block without negative impacts.

It should be noted that if the site-specific amendment is granted by Council to increase the building height to eight (8) storeys then an amendment to the existing Development Agreement will be required.

CONCLUSION:

Planning Staff are recommending that the Design Review Board support the proposal for an eight (8) storey multi-unit building consisting of affordable and market unit housing subject to the Design Reviewer’s recommendations outlined in their report dated October 26, 2023.

PRESENTER:



Laurel Palmer Thompson, RPP, MCIP
Planner III

MANAGER:



David Douglas Gundrum RPP, MCIP
Manager of Planning & Heritage

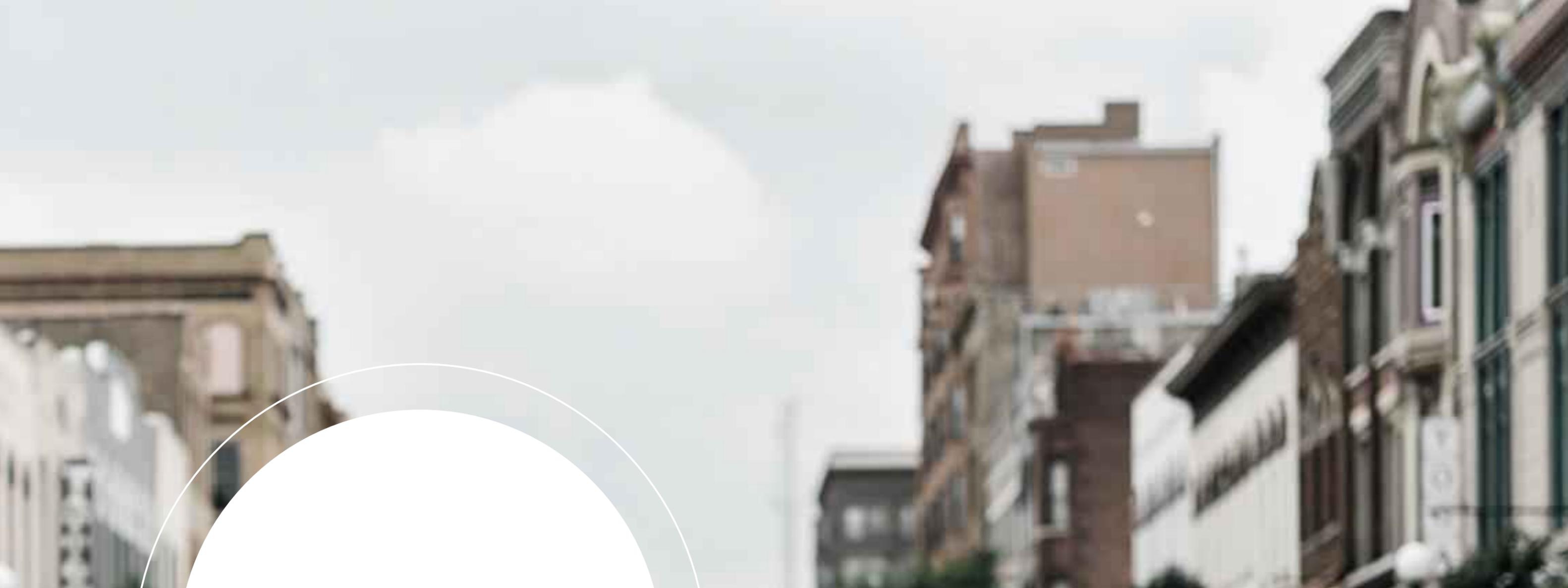
Attachment A

Design Review Submission Including Rational Paper

File: DESIGN-2023-30-OCTOBER 6B
199 GRAFTON STREET (PID# 342790)

OWNER: 102390 PEI Inc.
(c/o Tim Banks, APM Commercial Royal LePage)





THE
PORTHOUSE
Reimagined

APM  ROYAL LEPAGE
COMMERCIAL



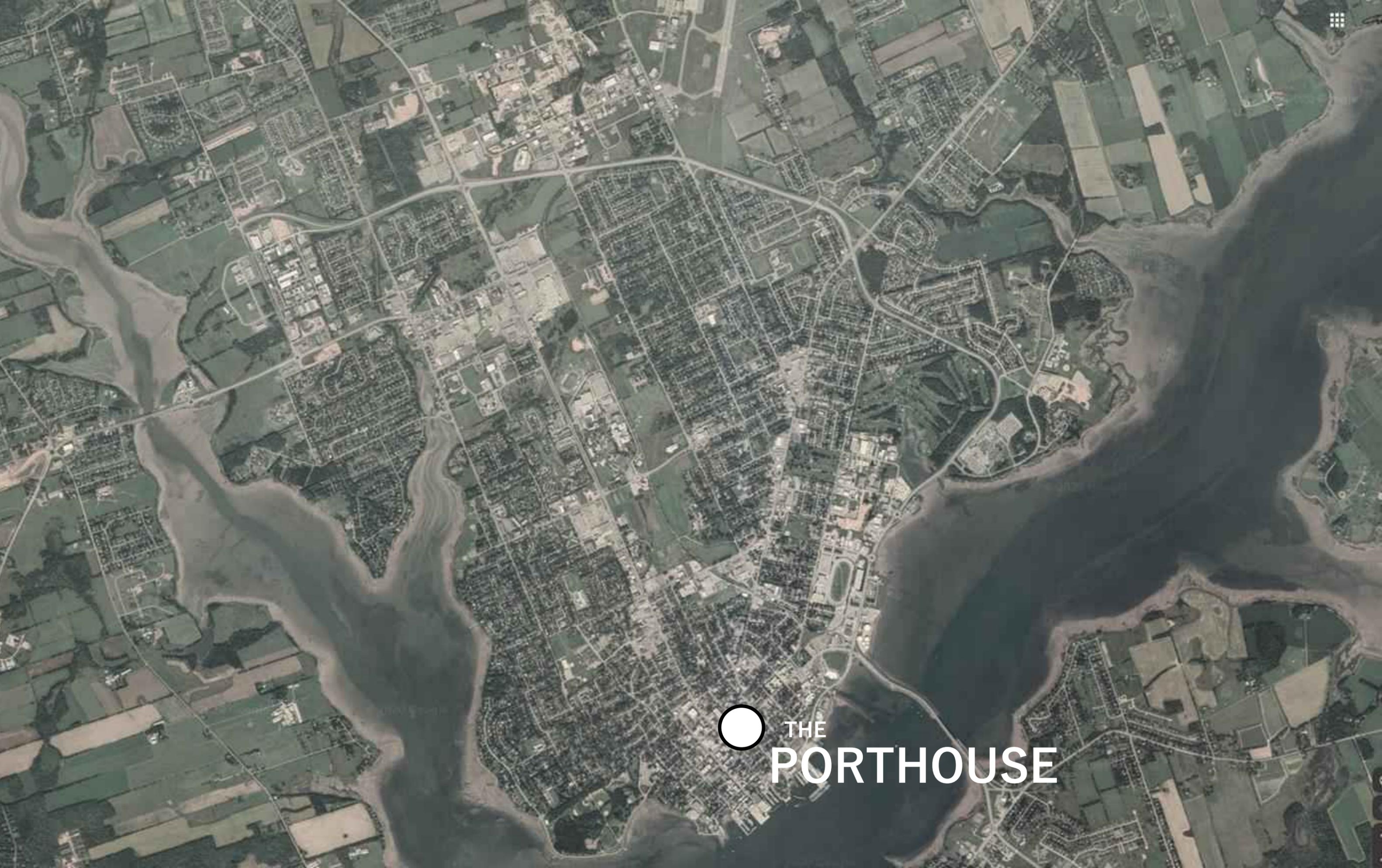
THE PORTHOUSE

The **PORTHOUSE** development is a unique and diverse urban housing project located in the heart of downtown Charlottetown, planned for young professionals, mature families and aging citizens looking to live and work in the downtown core.

Located on Prince Street in the center of the 500 Lot Area and only steps away from shopping, parks, services and finance, the Port House not only encourages a healthier mode of living, but also contributes to the cities social and economic growth while providing much needed housing in the downtown core.

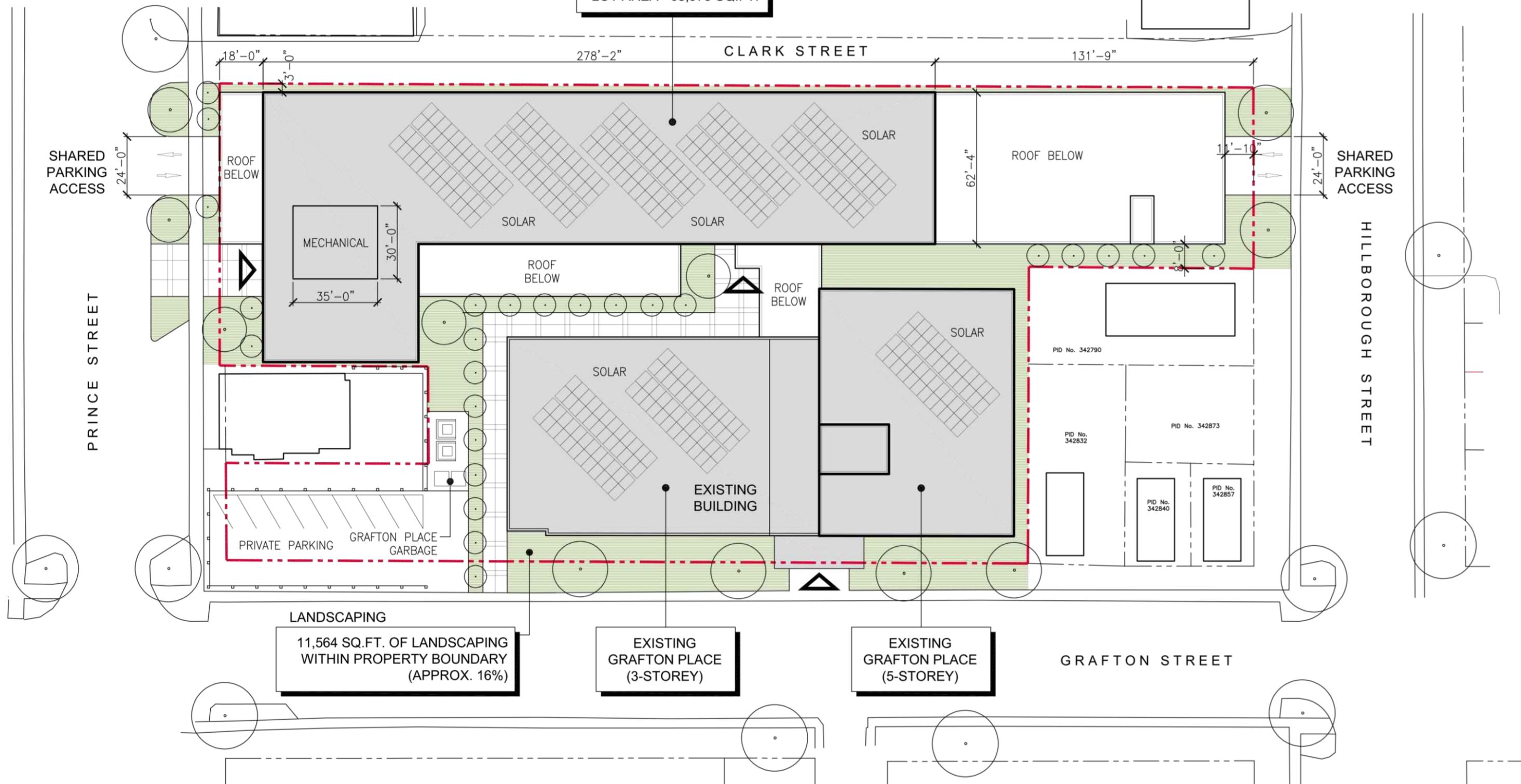
With only quality finishes in mind to compliment the area and features such as indoor parking, common amenity space, renewable energy and walkable access to all the amenities you will need, The Port House development is the first of its kind in the downtown core and a catalyst for what's to come.





THE
PORTHOUSE

**PORTHOUSE
8-STOREY
158 - UNIT APARTMENT
LOT AREA - 68,975 SQ.FT.**



**LANDSCAPING
11,564 SQ.FT. OF LANDSCAPING
WITHIN PROPERTY BOUNDARY
(APPROX. 16%)**

**EXISTING
GRAFTON PLACE
(3-STOREY)**

**EXISTING
GRAFTON PLACE
(5-STOREY)**

SITEPLAN - NOT TO SCALE

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SP1

Disclaimer* These drawings are preliminary only and subject to change based on final design, market pricing and product availability...



Disclaimer* These drawings are preliminary only and subject to change based on final design, market pricing and product availability...





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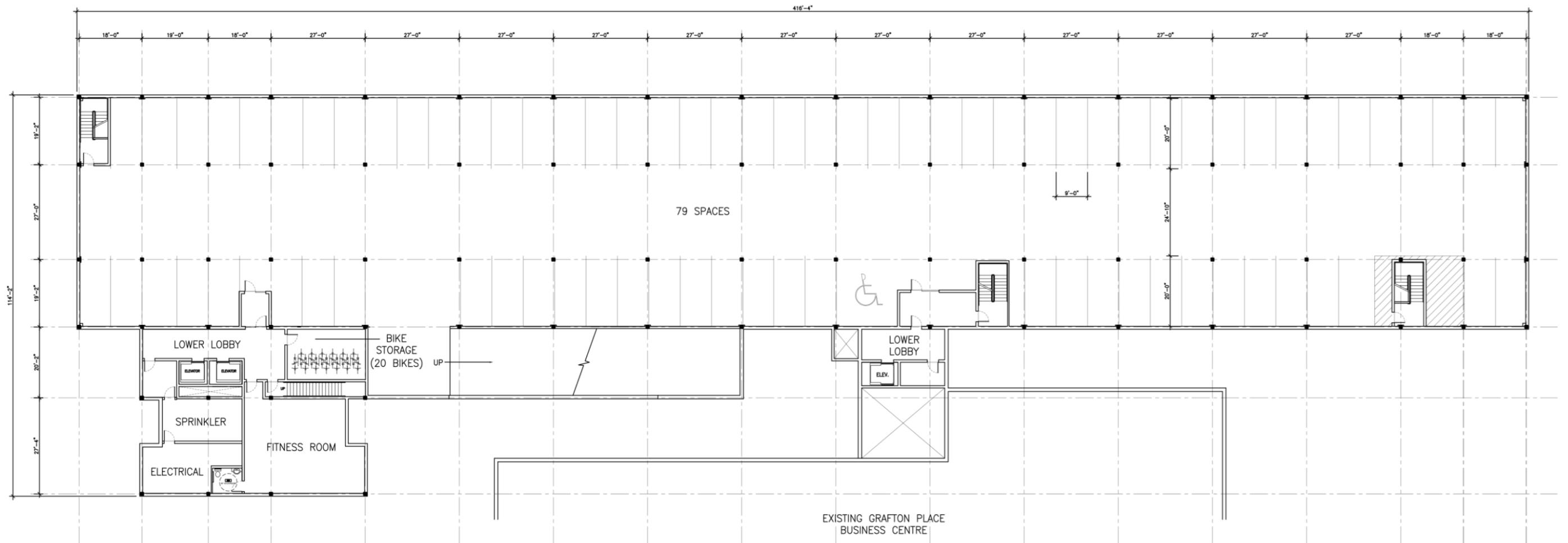


Disclaimer* These drawings are preliminary only and subject to change based on final design, market pricing and product availability...



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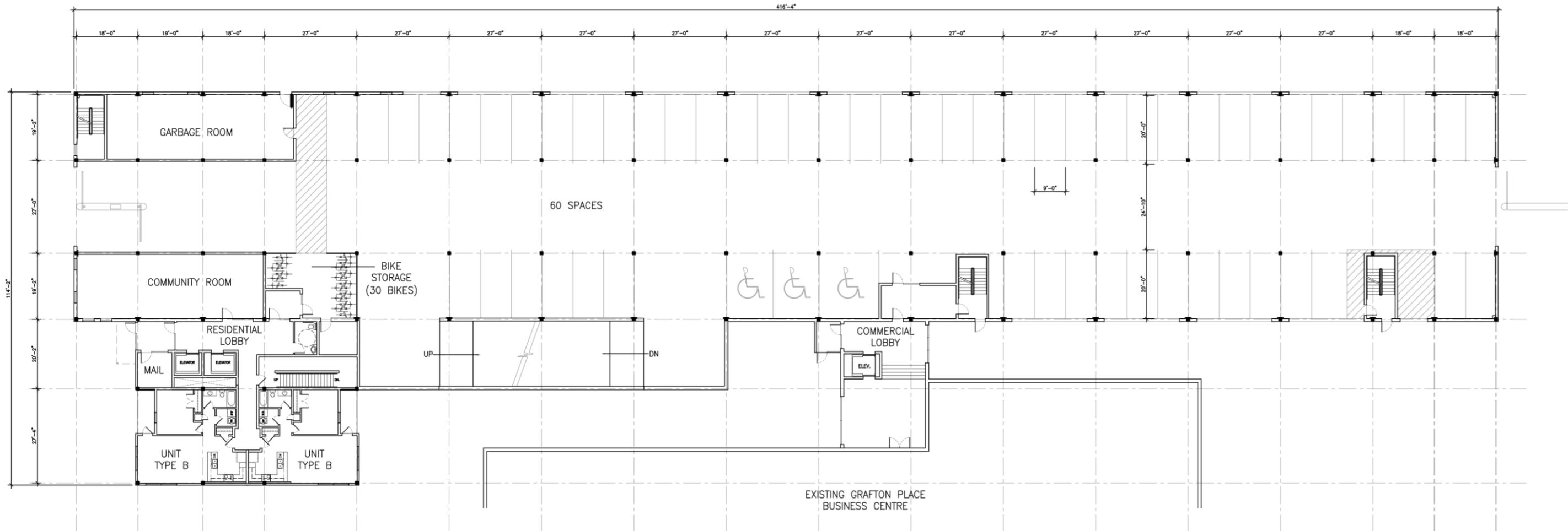


PARKING GARAGE AREA 32,855 SQ.FT.

LOWER LEVEL - NOT TO SCALE

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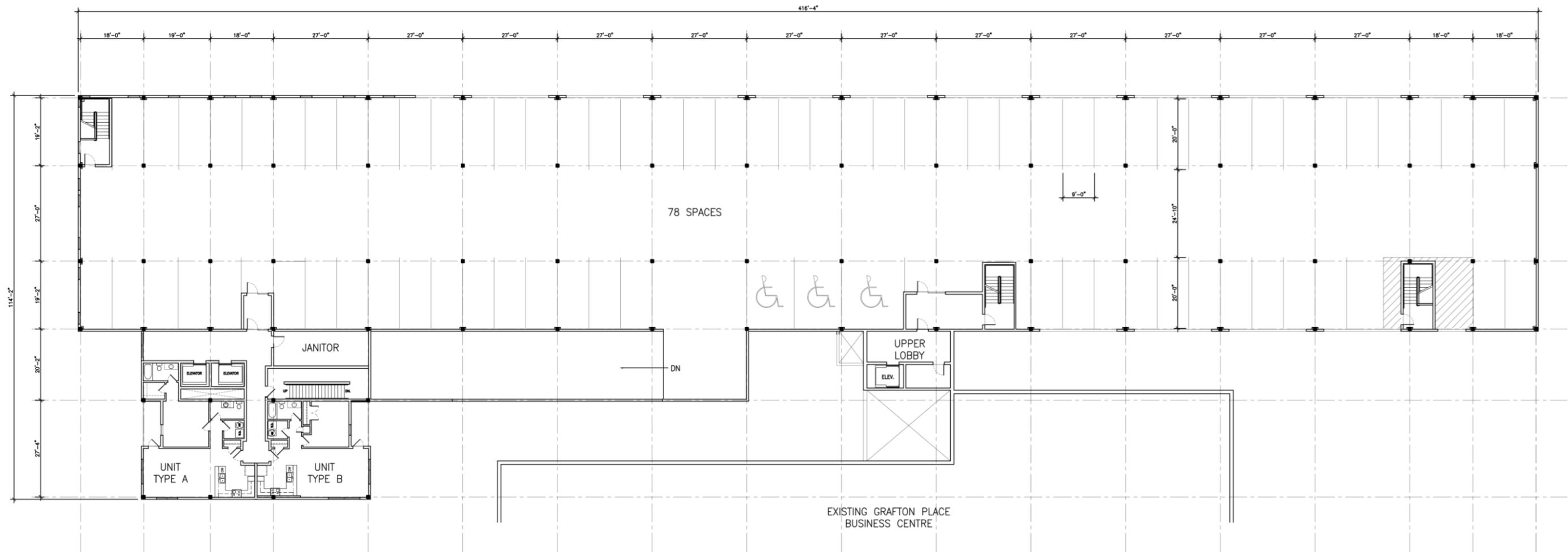


PARKING GARAGE AREA 29,855 SQ.FT.
 LIVING AREA 3,000 SQ.FT.
 (2-UNITS)

MAIN LEVEL - NOT TO SCALE

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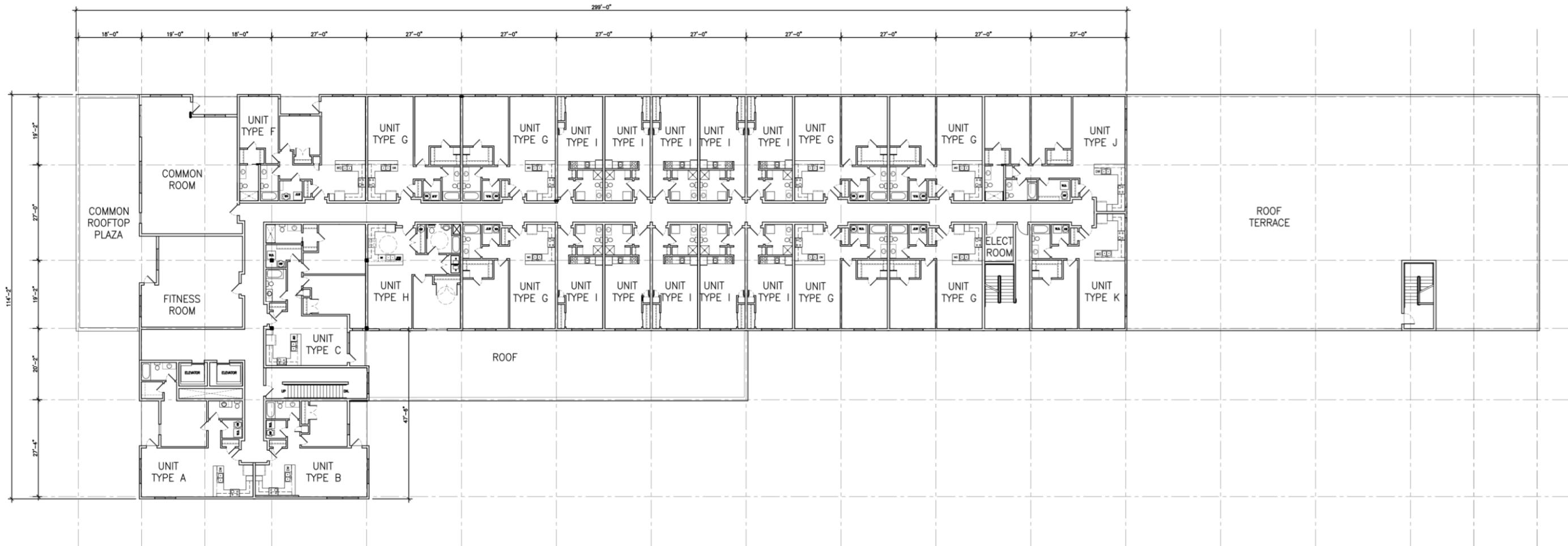


PARKING GARAGE AREA 29,855 SQ.FT.
 LIVING AREA 3,000 SQ.FT.
 (2-UNITS)

SECOND LEVEL - NOT TO SCALE

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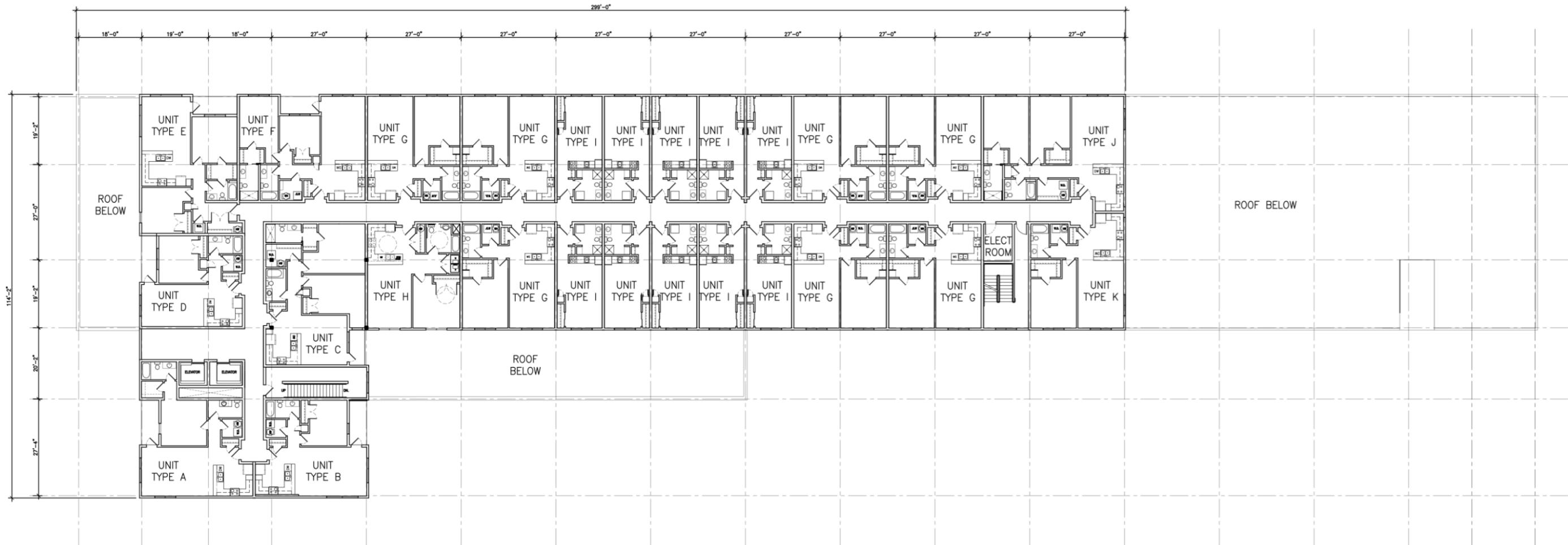


LIVING AREA 21,467 SQ.FT.
(24-UNITS)

THIRD LEVEL - NOT TO SCALE

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A-3



LIVING AREA
(26-UNITS PER/FLOOR)

21,467 SQ.FT. (PER/FLOOR)

LEVELS 4-8 - NOT TO SCALE

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A-4

Disclaimer* These drawings are preliminary only and subject to change based on final design, market pricing and product availability...



**“We Live Here...We are Proud of
What we Build Here”**

APM Commercial

August 23, 2023

Eleanor Mohammed
Chief Administrative Officer (CAO), City of Charlottetown
199 Queen Street
Charlottetown, PE
C1A 4B7

Subject: New development agreement for multi-unit apartment (Port House)

On behalf of our client, 102390 PEI Inc., APM Commercial is formally re-submitting our application to proceed with a new development agreement for the re-envisioned 8-storey Porthouse Development on the property of 199 Grafton Street, Charlottetown, PE (PID 342790).

Unfortunately, due to unforeseen circumstances that prevented us from securing a development agreement for the initial Porthouse development proposal, we were unable to begin this project as we originally expected. As a result of these significant delays, added costs related to inflation and interest rates have impacted the project to a point where it is no longer financially possible, and therefore we are now proposing a new 8-storey building with 126 market units and 32 affordable units for a total of 158 new units in the downtown core.

It is our understanding that to enable this development project there are a number of new variances and interpretations to the 500 lot area development standards and guidelines that need to be re-evaluated. APM Commercial is seeking to process these variances and the development agreement concurrently.

We look forward to your cooperation in quickly moving forward with the design review and public consultation process for this exciting new project that will revitalize vacant land in the downtown core, provide much needed sustainable residential development and will undoubtedly encourage and foster economic growth for not only the downtown core but all of Charlottetown.

Yours Truly,
APM commercial



Cain Arsenault

TABLE OF CONTENTS

1. PROJECT BACKGROUND
2. SITE CONTEXT
3. SITE CHARACTERISTICS
4. DEVELOPMENT PROPOSAL
5. ENVIRONMENTAL IMPACT
6. CONCLUSION

APPENDICES

- A. Renderings
- B. Site and Building Plans
- C. Topographic Survey
- D. Reference Photographs

1.

PROJECT BACKGROUND

This re-envisioned development is an exciting new 8-storey multi-unit residential building fronting on Prince Street and connected to the former Charlottetown Polyclinic. It has been redesigned with 6-storeys of residential apartments on top of a 2-storey parking garage at grade, with four additional residential units and a lobby at these ground floor levels, and finally an additional level of parking underground.

This new concept continues to focus on the property owner's desire to maximize the potential of this unique and underutilized downtown property, while responding to a growing need for more "downtown" housing that is both market and affordable in nature, and that has access to parking and indoor/outdoor amenity spaces.

In response to past dialogue with the planning department, City Council and ongoing comments from the general public, our following goals for the building still apply:

- Provide high quality urban living which is walkable and there for less dependent on motor vehicles.
- Generate a characteristic design that plays on different aspects of the surrounding neighbourhood.
- Effectively add to the exiting streetscape.
- Be sensitive to building mass where it is possible and most important.
- Provide more "downtown" multi-residential housing for all individuals, including families and seniors.
- Inherit higher than normal environmental efficiency standards.
- Enhance on-site parking.

With that being said, we are pleased to reintroduce the **Porthouse** Apartments, a well designed, unique and sustainable development that meets these and other important goals of the Official Plan.

This project would not be possible if it were not for the property owner's enthusiasm for high quality development and his desire to see revitalization in the downtown while maximizing the potential of the uniquely situated and underutilized property.

2.

SITE CONTEXT

199 Grafton Street is a parcel of land that was formerly developed as the Charlottetown Polyclinic and is now being adapted and rebranded as a new downtown professional centre that will be known as Grafton Place.

The site is located in the 500 Lot area which encompasses what many feel is the city's historical, cultural, civic and symbolic core. The area chronicles as a mixed demographic of local professionals, young families, and empty nesters taking up residence in single family homes and converted multi-unit developments with an increasing number of newcomers and islanders moving home from away.

The built environment directly surrounding our site can only be described as an assortment of commercial and residential development with a mix of retail, office, institutional and converted multi-unit residential uses of varying forms and styles both new and with historical character. Specifically, you will find the Maritime Electric head office, The Guardian building, Zion Presbyterian Church and St. Paul's Church, the former Charlottetown Polyclinic and an assortment of other big and small businesses and converted mixed-use homes.

When focusing indirectly around this site you will find even more of the same blend of commercial, institutional and residential development that mixes together and over the years has formed an authentic and lively neighbourhood. This also includes the downtown core, where only a block away from our site, you find an even greater diversification of historical and new developments merged together with altering scale and architecture.

With taller buildings such as the Holman Grand, Homburg Financial Tower, BDC Place, Maritime Electric Building, National Bank Tower and numerous others all in close proximity to our site, there is no reason to believe our 8-storey proposal will not fit-in and contribute to the area in the same way these other tall buildings have done.

(See appendix D – Reference Photographs)

3.

SITE STATISTICS

Location

199 Grafton Street (Former Charlottetown Polyclinic parking lot).

Physical Characteristics

The property is approximately 68,973 square feet in size, with 330 feet of frontage Along Grafton Street, 75 feet of frontage on Hillsborough Street, a combined frontage of 156 feet along Prince Street (Divided by PID No. 343053) and 428 feet along Clark Street, which can only be described as a one-way service lane at the back of converted mixed-use development along Kent Street.

Access Points

The existing main parking access is from Hillsborough Street with access from Grafton Street to a smaller private parking area.

Existing Use

The subject property currently consists of a single 5-storey building focussing on clinical office, retail and related services with a large parking area accommodating 118 spaces.

Planning Designation and Zoning

The subject property is currently zoned Downtown Mixed-use Neighbourhood (DMUN). Apartments and office buildings, as well as parking structures exist and shall continue to be permitted in Downtown Mixed-Use Neighbourhood areas with careful design consideration.

(See appendix C – Topographic Survey)

4.

DEVELOPMENT PROPOSAL

As stated, this proposed development responds to a growing need for more housing in the downtown core with access to parking and indoor/outdoor amenity spaces. However due to parking requirements, site restrictions, increasing costs of construction and antiquated by-law requirements and guidelines, the city's housing needs are not being met at an acceptable level. As a result, we are proposing an 8-storey multi-unit residential apartment building that provides the density needed to offset higher costs, provides parking and delivers a significant number of much needed housing units to the downtown core.

It is our stance that providing such elements as distinct building character, additional green space, access to more affordable housing in the downtown core, a generous setback from Prince Street with open green space, sustainable building features, adequate parking and direct access to community services (via Grafton Place), should be more than enough incentive and public benefit to mitigate any concerns relating to the variances we require.

Having identified no significant commonality or distinction surrounding the area, the design for the proposed building adapts to what we feel is appropriate for this site with distinct proportions through clearly defined vertical and horizontal forms that break up the scale of the building. Each volume is then characterized with a different material or color and finally recessed balconies, along with exaggerated cornices further enhance and animate the building along Prince Street where it is most important.

The new 3-storey ground floor street wall will be clad in brick or stone and broken up with panels of glass to give a more relevant look. The remainder of the building which is significantly setback from the street will be high quality metal panels and prefinished horizontal siding with a well-balanced fenestration of windows. The colors will be generally "warm" in character and thoughtfully considered with respect to adjacent developments and style.

Our plan is to return these finishes on each side facing Prince Street for a short distance, then transition to only prefinished horizontal siding where the building is less visible to the public eye. Reveals and different tones will be incorporated with a good balance of windows to give some added character along the sides and break up the mass. The back portion of the building facing Hillsborough Street will be treated similarly to the elevation facing Prince with the same consideration of quality and massing along that street.

We will be engaging a third-party architect for this project and giving them some creative leadership to further enhance finishes and materials as they see necessary, and we expect that any material changes made after the fact will be accepted without prejudice or any further public consultation that would further delay or impede this development.

The breakdown of our building is as follows:

Unit composition

	Unit Count	Average Unit Size
Bachelor	60	400 sq.ft.
One Bedroom	75	800 sq.ft.
Two Bedroom	23	1200 sq.ft.

Vehicle parking

The total number of parking spaces provided in the proposed project is 232 spaces, almost doubling the current capacity. 109 spaces will be dedicated to the residents of the apartment plus 5 barrier free spots and the remaining 118 spaces will be offered to the commercial tenants and public using the Grafton Place Business Centre that current have access to this amount.

Bicycle parking

Class “A” bicycle parking will be incorporated along with a space for bicycle repair.

Amenity space

The following are some amenities provide:

- Private balconies
- Common rooftop patio
- Community room
- Fitness Centre
- Bicycle storage and repair area
- Car wash bay
- EV charging stations
- Direct access to services via Grafton Place

(See Appendices A & B – Renderings and Building Plans)

5.

ENVIRONMENTAL IMPACT

With the common concern of global warming so prevalent in all our minds, one of the major goals of this project is to deliver a building that has as little impact on the environment as possible. We are approaching this development with an open mind to renewable resources and modernized mechanical systems which result in a highly efficient and sustainable development.

Some design features that we have considered are:

- Increased insulation and thermal comfort.
- Better indoor air quality and circulation.
- Energy efficient HRV systems.
- PV solar panels on both new and existing buildings.
- LED lighting throughout.
- Highly efficient glazing.
- Low flow toilets.
- Water meters for each unit.
- Bicycle storage and repair.
- Public and private EV charging stations

While this project is not fully net zero or passive to their full definition, we are confident that we are providing a development that exceeds the expectations of the National Building Code and Energy Code of Canada and will have positive impact on the environment.

6.

Conclusion

APM Commercial is very excited to present this adaptive development proposal that regenerates this underutilized piece of land and establishes a significant amount of new “downtown” multi-unit residential apartments. In considering the built form and character of the area we feel our proposal enhances the neighbourhood with thoughtful urban character and architectural style.

Prince is a broad, vibrant and energetic street that demonstrates an eclectic mix of form, scale and architecture that is both historic and modern. Recognizing this, we want to develop something that does not mimic the built environment but instead stands out visually and makes an impression through high quality environmental urban design and quality detail standards.

We are strong believers that professional design excellence along with open dialogue and an open mind to change will serve as the catalyst for revitalization and livability of the 500 Lot Area and Charlottetown as a whole. We look forward to working together with the City of Charlottetown in pursuit of this unique and promising in-fill development that will not only contribute to the 500 Lot Area, but will also bring significant jobs, tax base and choice to the existing and future residences of the city.

We look forward to your comments and cooperation as we move forward with this exciting new proposal and should you have any questions, please feel free to contact us.

Thank you,
APM commercial



Cain Arsenault

Appendix A – Renderings







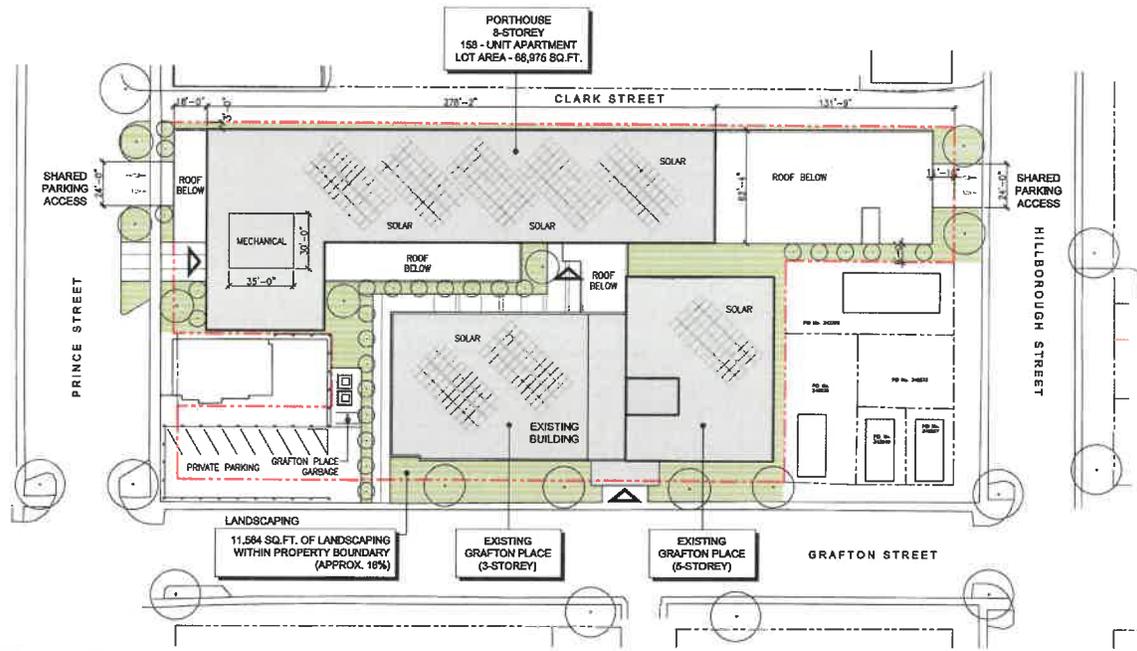








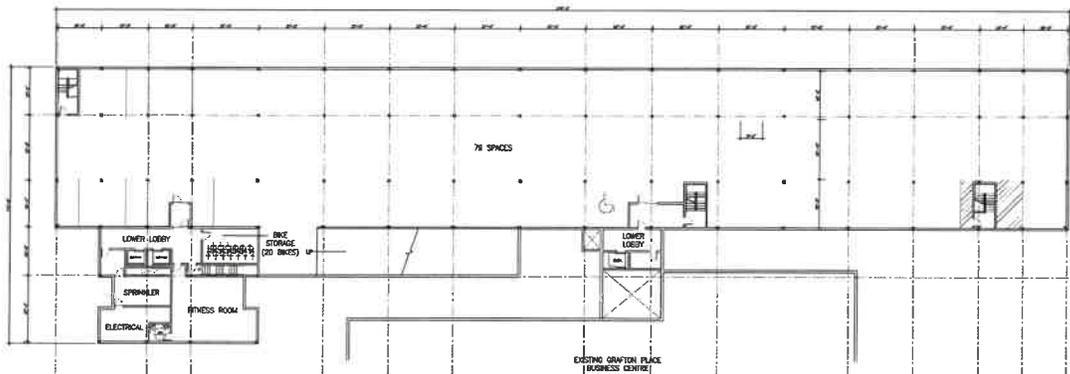
Appendix B – Site and Building Plans



SITEPLAN - NOT TO SCALE

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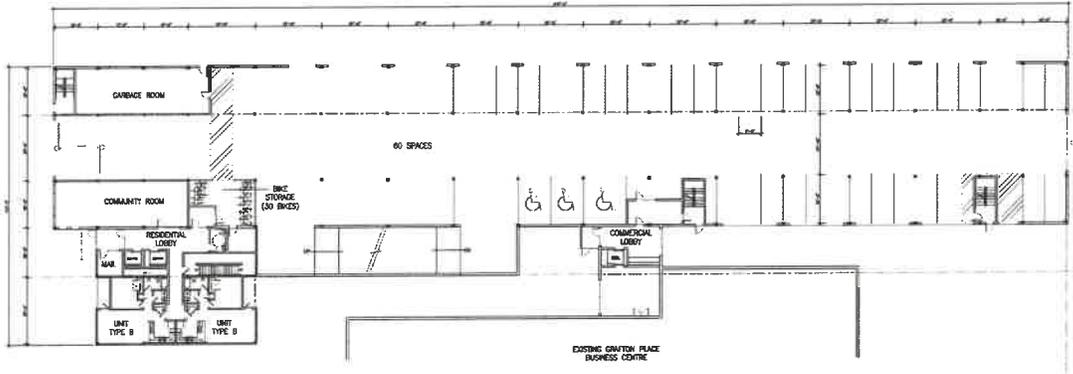


PARKING GARAGE AREA 32,855 SQ.FT.

LOWER LEVEL - NOT TO SCALE

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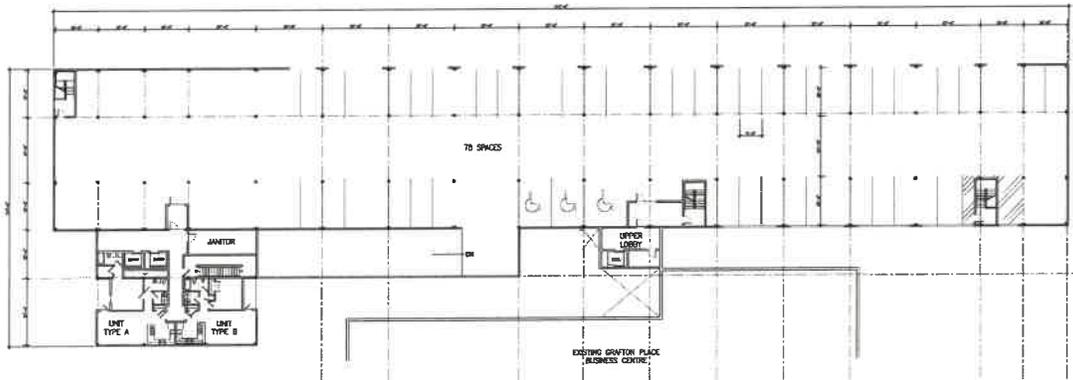


PARKING GARAGE AREA 29,855 SQ.FT.
LIVING AREA 3,000 SQ.FT.
(2-UNITS)

MAIN LEVEL - NOT TO SCALE

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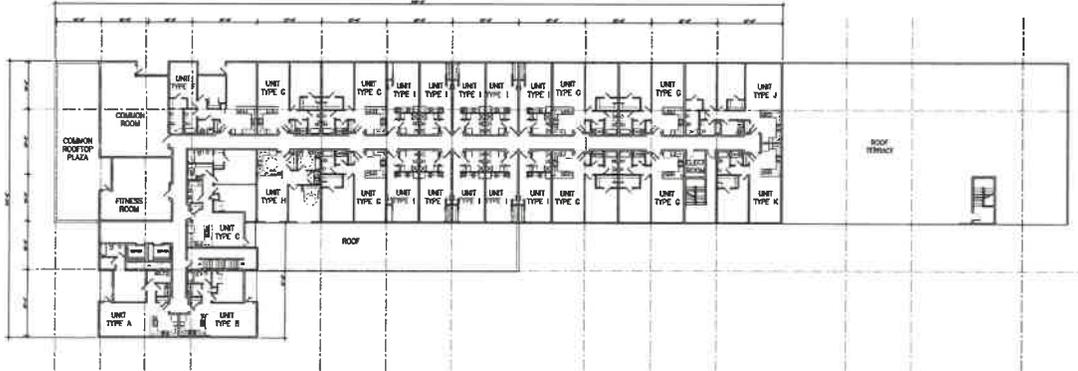


PARKING GARAGE AREA 29,855 SQ.FT.
LIVING AREA 3,000 SQ.FT.
(2-UNITS)

SECOND LEVEL - NOT TO SCALE

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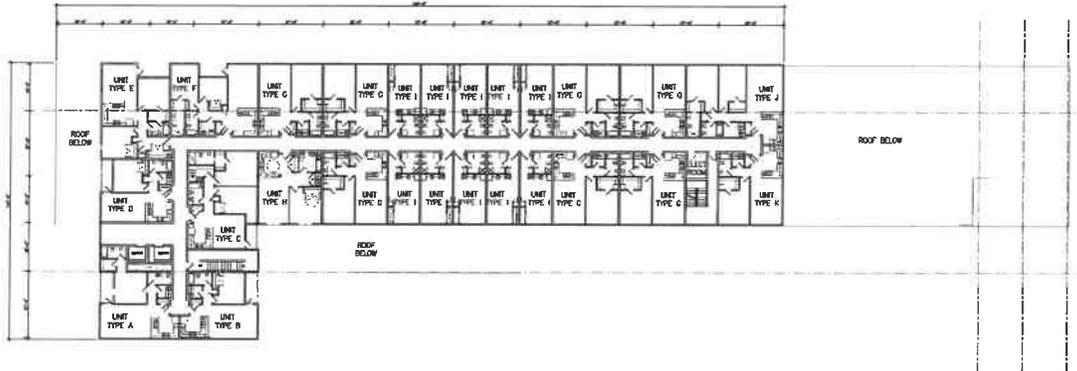


LIVING AREA
(24-UNITS) 21,467 SQ.FT.

THIRD LEVEL - NOT TO SCALE

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A-3



LIVING AREA
(26-UNITS PER/FLOOR) 21,467 SQ.FT. (PER/FLOOR)

LEVELS 4-6 - NOT TO SCALE

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A-4

Appendix C – Topographic Survey

Appendix D – Reference Photographs











Attachment B

External Design Reviewer Comments October 26, 2023

File: DESIGN-2023-30-OCTOBER 6B
199 GRAFTON STREET (PID# 342790)

OWNER: 102390 PEI Inc.
(c/o Tim Banks, APM Commercial Royal LePage)





16 Moore Drive
Murray River PE COA 1WO
902.330.2398
greg@munnarchitecture.ca

October 26th, 2023

Laurel Palmer Thompson
City of Charlottetown Planning and Heritage Department
70 Kent Street, Charlottetown PE
902.629.4108

DESIGN REVIEW of
199 Prince Street, Charlottetown, PE

Dear Laurel,

The following Design Review has been prepared for the proposed development for the 199 Prince Street property, utilizing the design documents received from you on October 16, 2023.

My understanding as a Design Reviewer is that I am to present my observations and recommendations, focused on the effect that a new or renovated structure may have on the heritage value of its context. Sections 4 and 5 of the 500 Lot Development Standards & Design Guidelines, Heritage Preservation Bylaw (amended March 15, 2019), plus Sections 3.14 DESIGN REVIEW and Section 7 DESIGN STANDARDS FOR THE LOT 500 AREA, of the ZONING AND DEVELOPMENT BYLAW (amended March 28, 2023) have been utilized to guide my observations and recommendations.

The proposed project will be sited in the DMUN (Downtown Mixed-Use Neighbourhood) zoned area. The design was previously approved for a six-storey building. This review will focus on the impact of two added floors, to eight storeys, at a total height of 87'-8" from the main floor level on Prince Street, requiring a site-specific variance as the Bylaws for this block limit building height to 39'-5".

This review will however revisit the architectural and character defining features of the proposed building as potential discussion points in consideration of allowing such a tall building to be built in our unique and historic city.

1. SITING AND ORIENTATION

The character of Prince Street at this block is currently a mixture of residential, religious, and commercial buildings, ranging in height from one to five storeys. This site is located in the dense downtown area of the city, and infill at this block will be a welcomed addition to strengthen the density of the area. An open underdeveloped lot is like a smile with a missing tooth. Moving east, the city quickly evolves from commercial to lower and less dense residential building stock. It is appropriate for the "front" and main entrance of the development to be facing Prince Street, and the lower, humbler mass of the building facing the quieter Hillsborough Street.

RECOMMENDATIONS: No recommendations

2. PARKING ACCESS AND GARAGES

The proposed design will provide much needed parking in the downtown. The previous design review made suggestions for the face of the parking garage on Hillsborough Street. I would offer that further development of the design will be beneficial for the streetscape of Hillsborough Street.

Section 7.11.3.b. of the Charlottetown Zoning and Development Bylaw regarding parking structures states that “The front façade shall be designed to conceal the parking levels and give the visual appearance of a multi-Storey Building articulated with bays and window openings”.

RECOMMENDATIONS: The brick faced, three-storey mass of the Prince Street façade of the proposed building contains traditionally proportioned vertical window openings. These openings are placed in a well composed and dynamic pattern of solid and void spaces, that sometimes do align vertically, and sometimes don't, as they reflect the configuration of the spaces within. It is a successful contemporary interpretation of a traditional form. It is recommended that the Hillsborough face of the parking garage carries the same aesthetic of openings and voids, with vertical openings that match the size and shape of the Prince Street windows, without glass, composed across the façade, instead of the wide openings. It is recommended that the metal panels are removed from the design and the material is limited to brick. The goal and intent of the Bylaw is to make the garage facing the street seem more for people than cars, and this would tie the two ends of the building together architecturally, as well introduce a human scale to the façade that will better relate to the neighbouring houses.

3. MASS AND SCALE

The proposed three storey mass facing the street at Prince Street is appropriately scaled in height and width, that will enhance the character of this street. The three-storey block is better proportioned than the two-storey block from the first proposal. Similarly, the brick faced of the Hillsborough Street side to the building is appropriately scaled for this street and its context.

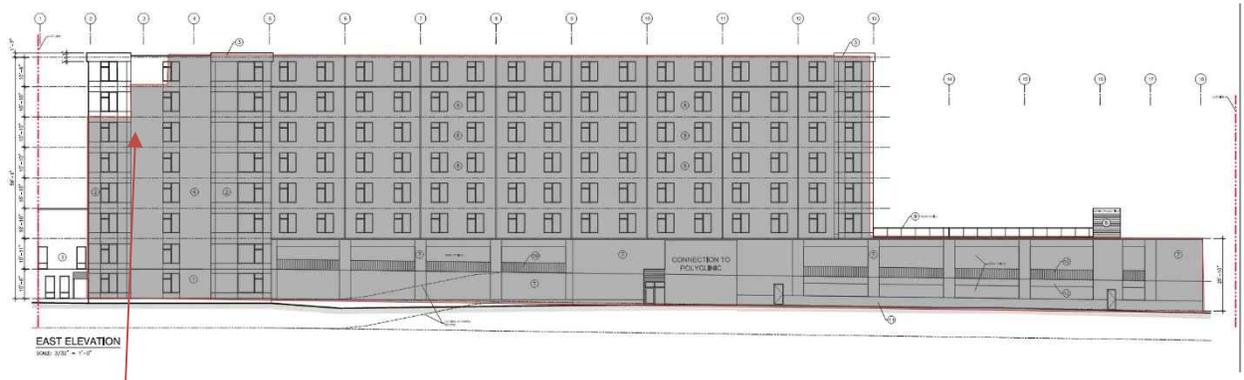
As noted in the introduction, 6 floors have been approved by variance, and the crux of this application is to request a variance to go to eight floors. The proposed height is 87'-8", which is 27'-1" taller than the allowed maximum bonus height in the city. 30.3.2.c.iii Bonus Height Development Standards suggested that a 45-degree angular plane is implemented to achieve the bonus height.

The tall mass is set back from both front streets with a three-storey brick massing at the Prince Street side and the two-storey brick massing at the Hillsborough Street side. Therefore, the tall massing of the proposed building will not loom directly over these principal streets. The former Polyclinic building will mostly shield the view of the building from Grafton Street. The building will be barely visible from Kent Street, except looking at it from the corner of Prince and Kent, but it will be at a distance. The concern over mass and scale is how the building meets Prince Street, and the bigger issue of the precedence that allowing a building of this height to be built in the Lot 500 zone.

RECOMMENDATIONS: Two options to consider:

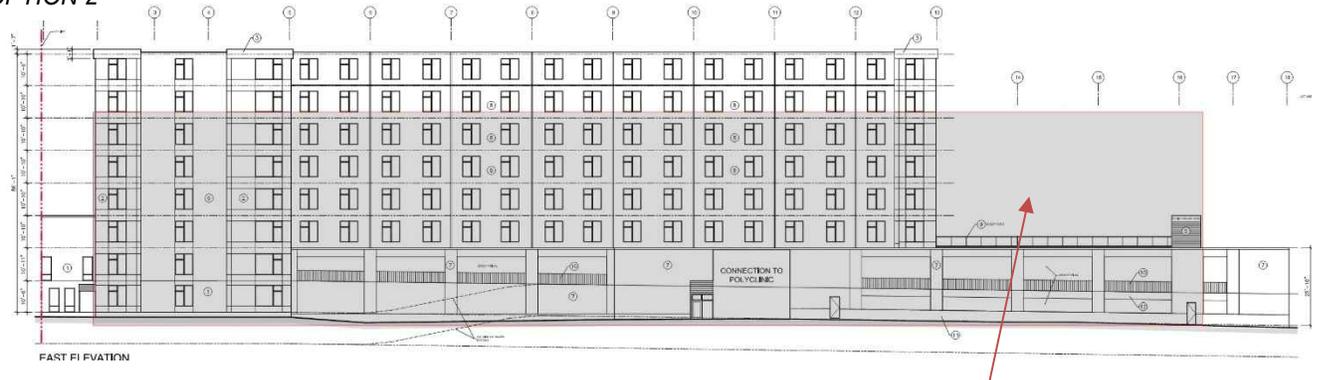
1. is recommended that the top two floors of the 8-storey proposal are stepped back to achieve the 45-degree angular plane bylaw. If this option is not desirable, then a variance will be required to override the Bonus Height Development Standards regarding the required 45-degree angular plane setback.
2. Extend the east end of the building closer to Hillsborough Street, to compensate for the units lost if the building were to stay six storeys, see image top of next page. The shaded area would potentially provide an equal number of units without going higher than six storeys.

OPTION 1



Grey area shows proposed 45-degree angular setback of the 7th and 8th floors

OPTION 2



Grey area is potential 6 storey building, extending further to the east to compensate for units lost from the 7th and 8th floors

4. ROOFS

Flat roofs are traditional for commercial and multi-unit buildings in the downtown and are appropriate for this development. This roof will provide for the inclusion of a large solar array which I am very happy to see will be included in the development.

RECOMMENDATIONS: No recommendations

5. PORCHES AND ENTRIES

The proposed two-storey canopy on Prince Street is appropriately scaled for the building and is the clear point of entry to the building. The tree-like post supporting the roof softens the geometry of the building and will be a welcoming feature at the entry.

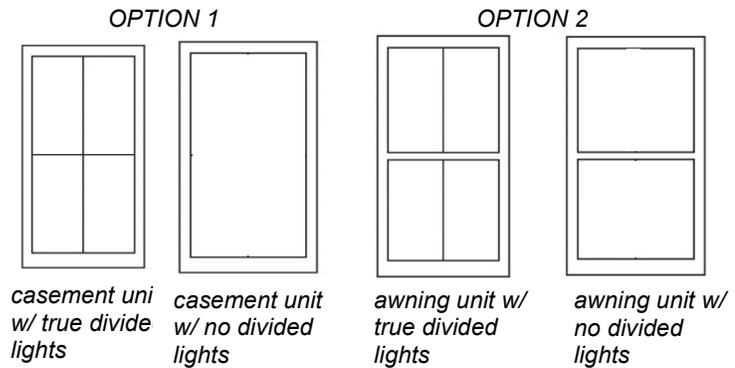
RECOMMENDATIONS: No recommendations

6. WINDOWS

7.5 of the Zoning and Development Bylaws deals with windows in the 500 Lot zone. The vertical proportions of the window units of the proposed building are in keeping with the Standards and Bylaws, and those units with large picture windows are in scale with the building and will provide great views and a lot of natural light into the units.

Architectural speaking, the units that have a small opening at the top and larger at the bottom, which suggests awnings or casements, is a feature that is currently popular, but in this reviewer’s opinion, will become dated in the near future. The traditional window style of Charlottetown is double hung or single hung units, most commonly two-over-two. As it is in fashion, something is in style, then not, but then becomes classic. The preferred profile is four equal sized windowpanes within the vertical proportioned frame, see images below.

RECOMMENDATIONS: Option 1: The small operable awning/ over larger picture unit windows should be replaced with either full height casement windows, with cross muntin true divided lights, or no muntin bars at all. Option 2: The stacked awning panes are to be of equal size to be more in keeping with the aesthetic of the traditional window stock of Charlottetown. Double or single hung windows are not recommended as this will detract from the contemporary aesthetic of the building, but the cross divided lights or equally proportioned awnings will be arguably more modern than the proposed windows, and more appropriate to enhance the character of Charlottetown and its historic context.



7. MATERIALS AND FINISHES

In the opinion of this reviewer, the Charlottetown Lot 500 zone is traditionally a wood, brick, and stone City. The use of these materials is encouraged. However, metal is a contemporary building material, and is appropriate in certain instances to define a building as contemporary, as contrasting quality materials is acceptable as to not confuse historic/ period structures with new. However, the complete rejection of the use of any traditional materials will detract from the context of the building and the traditional building stock that new construction should subtly reinforce.

The brick of the Prince Street three-storey block, and the Hillsborough Street façade, are appropriate and attractive. Brick is a relatable human scale material, and it is important to be of relatable materials at the street level of any building.

The generally warm but lighter colours of the metal panels and siding is thoughtfully distributed around the building to again, provide human scale to such a large building.

The wood grained cladding would typically be problematic, as 7.4.5 of the Zoning and Development Bylaw states that “The appearance of building materials shall be true to their nature and should not mimic other materials.”. However, in this case, the wood grained metal siding with hidden fasteners seems appropriate, as there are other elements of the design that takes tradition not the future, and the use of this material is in keeping with this aesthetic.

RECOMMENDATIONS: No recommendation

8. LANDSCAPING

Section 7.9.1 states that a landscaping area shall be provided between the sidewalk and the front façade of the building, where appropriate for a continuous streetscape. This has been provided for in the proposed design.

RECOMMENDATIONS: No recommendations

SUMMARY

The proposed development will provide much needed housing, and I am encouraged by increasing the number of affordable units from 12 in the initial submission, to 32 proposed in this revised design. However, I must stress that the purpose of this review is to also consider the long view of the impact of this development on the city. Every new building and development sets precedence and must be thoughtfully considered as to its impact on the character and context of the city. The unprecedented building boom seen in Charlottetown over recent years has generally been positive for the city but also means that review and consideration of new developments must be all the more diligent to ensure the City will retain its unique character, built over the past 2 ½ centuries, to remain attractive for growth and prosperity for future generations.

The following is comment on the application letter from APM Commercial to Eleanor Mohammed, dated August 23, 2023.

On page 4 of the letter, last paragraph, several tall buildings were listed as contributing to the City, such as the Holman Grand and Homburg Financial Tower, and that the proposed development for 199 Prince Street will contribute in the same way as these buildings which have set precedence for tall buildings in Charlottetown. It is however the opinion of this reviewer that these buildings detract from the character and scale of the Lot 500 area of Charlottetown. Part of that opinion is based on their rejection of the traditional and character defining building materials and finish, but more importantly, the scale of these buildings that interrupt the low and varied skyline of roofs, chimneys, spires, and trees that define Charlottetown as the place that people from all over the world want to visit and move to. Density and diversity should be increased in the Lot 500 Area, but there are creative ways to achieve this without diluting the very character that attracts people in the first place.

I recommend approval of the design provided that the recommendations are implemented.

Sincerely,

A handwritten signature in blue ink, appearing to read "Greg Munn". The signature is fluid and cursive, with a long horizontal stroke at the end.

Greg Munn, Architect,
President, Munn Architecture Inc.
AAPEI President, MRAIC, ROAC PEI Delegate, APT PEI Delegate

Attachment C

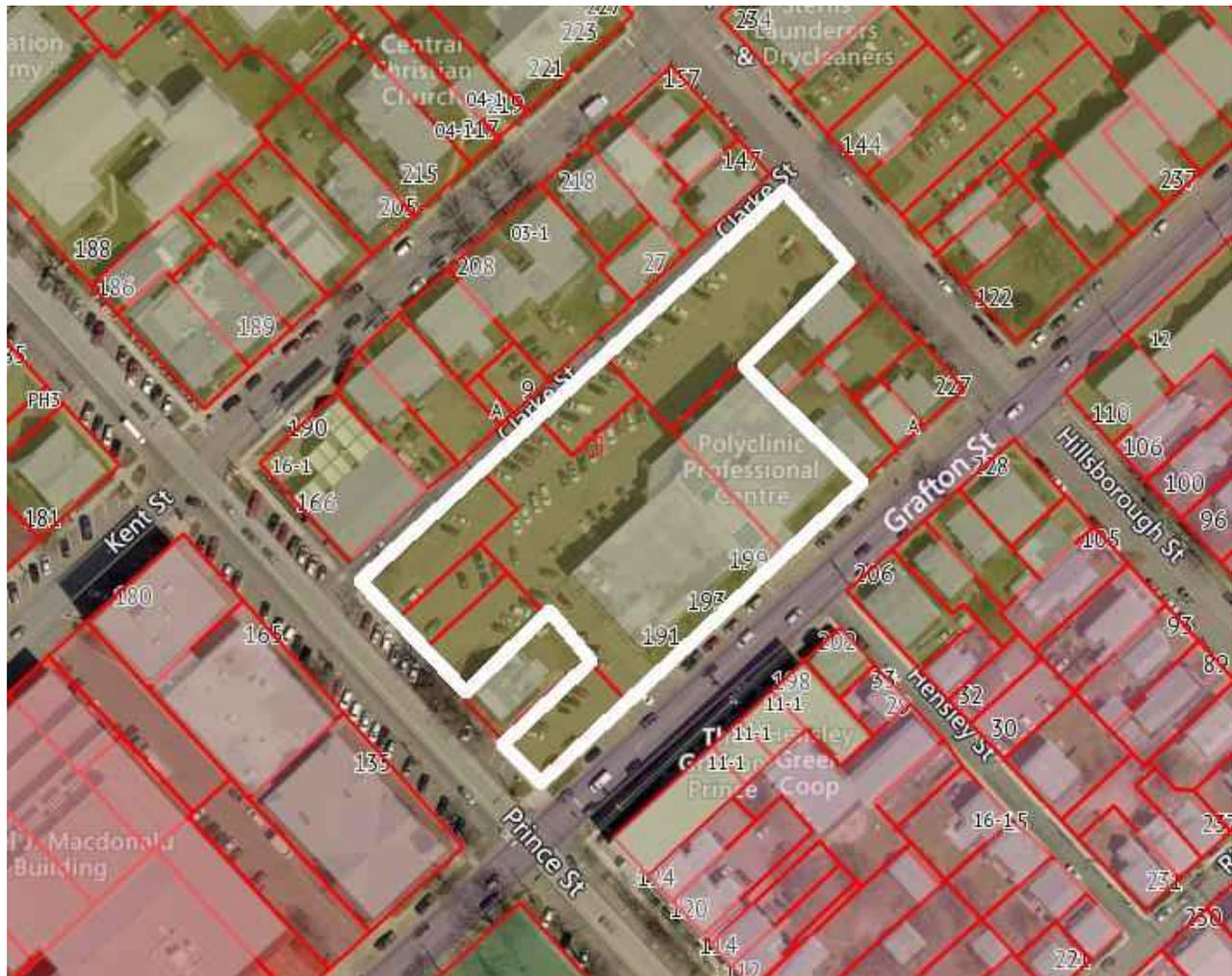
Design Review Submission February 21, 2021

File: DESIGN-2023-30-OCTOBER 6B
199 GRAFTON STREET (PID# 342790)

OWNER: 102390 PEI Inc.
(c/o Tim Banks, APM Commercial Royal LePage)



Attachment A – GIS Map



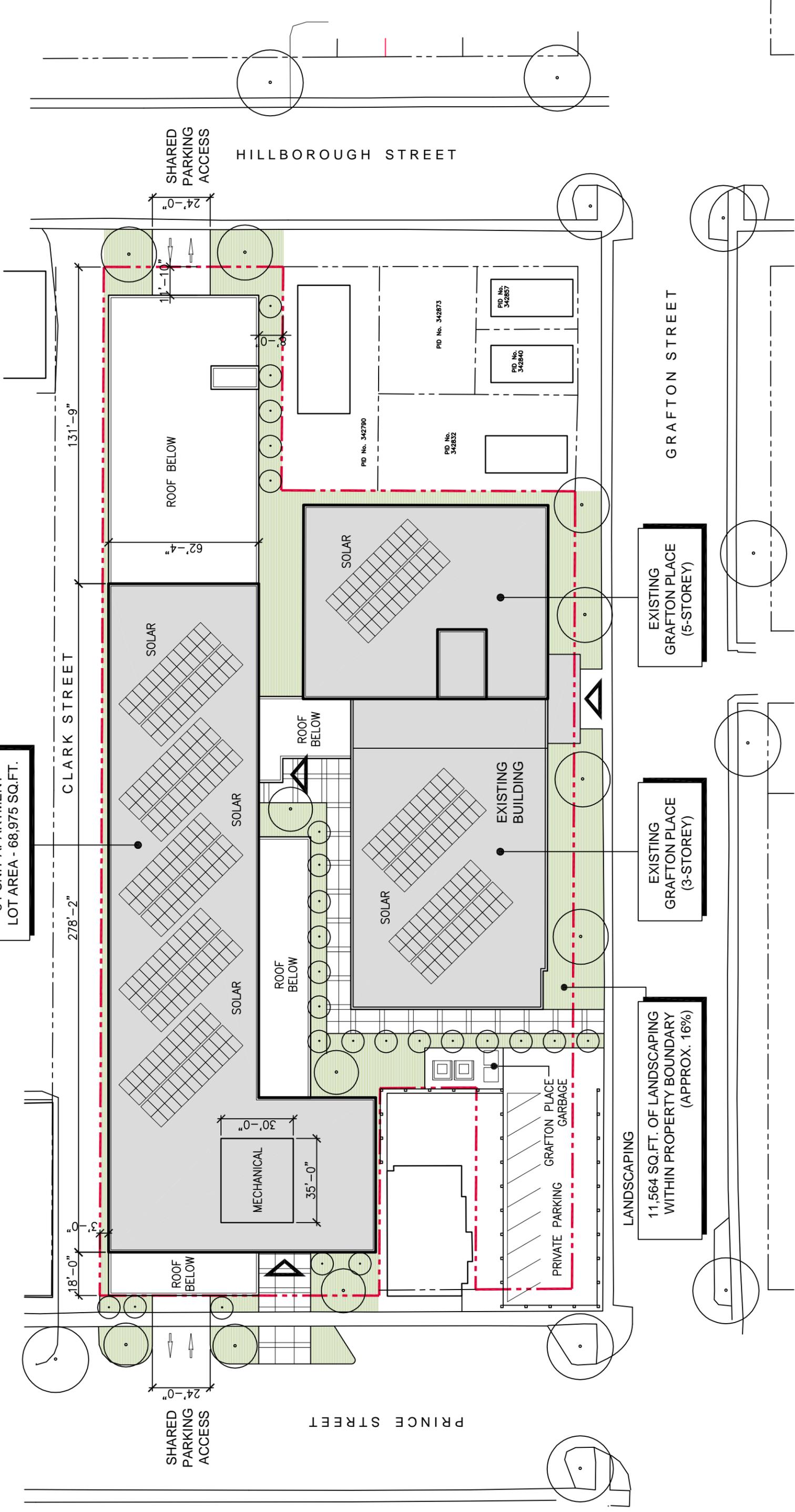
<p>SITE SPECIFIC EXEMPTION File: PLAN-2021-06-B-5 199 GRAFTON STREET (PID# 342790)</p> <p>OWNER: 102390 PEI Inc.</p> <p>APPLICANT: APM Commercial Royal LePage</p>	 <p>CHARLOTTETOWN Planning & Heritage Department</p>
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Attachment B

Site Plan

<p>SITE SPECIFIC EXEMPTION File: PLAN-2021-06-B-5 199 GRAFTON STREET (PID# 342790)</p> <p>OWNER: 102390 PEI Inc.</p> <p>APPLICANT: APM Commercial Royal LePage</p>	 <p>CHARLOTTETOWN Planning & Heritage Department</p>
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**PORT HOUSE
6-STORY
84-UNIT APARTMENT
LOT AREA - 68,975 SQ.FT.**



SITEPLAN - NOT TO SCALE

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PORT HOUSE APARTMENTS
Charlottetown, PE - April 01, 2021 (Rev.#2)

SP1

Attachment C

Architectural Concept Plans

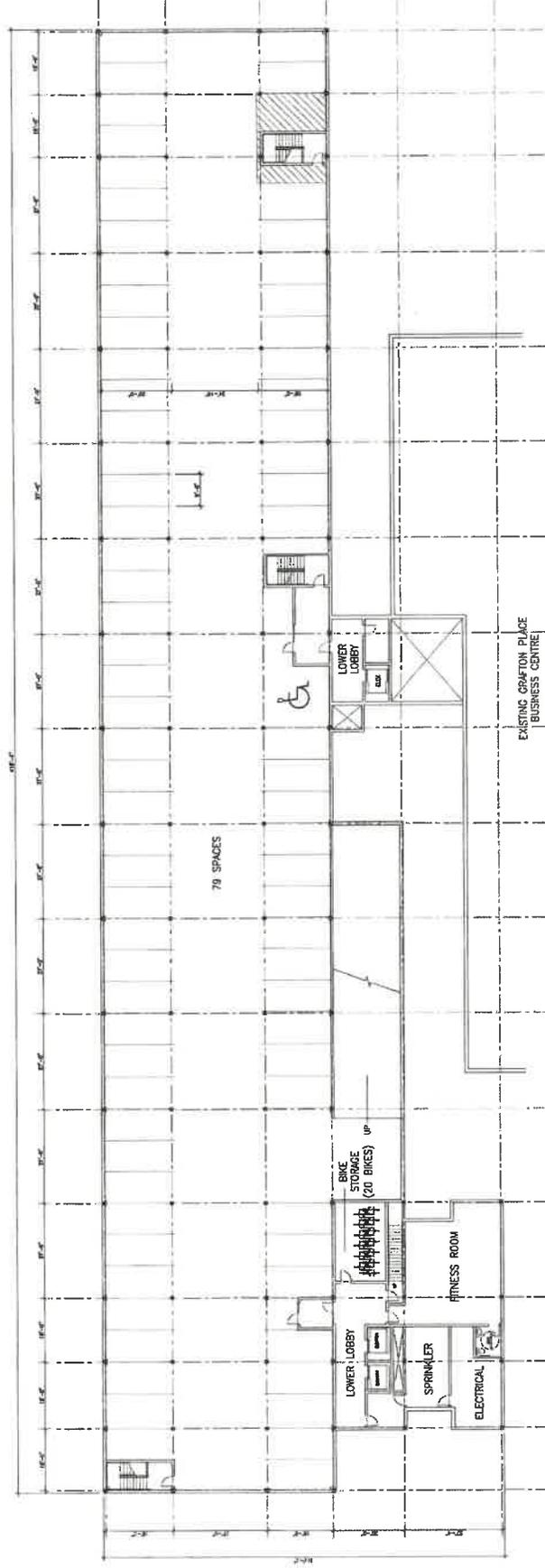
SITE SPECIFIC EXEMPTION

File: PLAN-2021-06-B-5
199 GRAFTON STREET (PID# 342790)

OWNER: 102390 PEI Inc.

APPLICANT: APM Commercial Royal LePage





PARKING GARAGE AREA 32,855 SQ. FT.

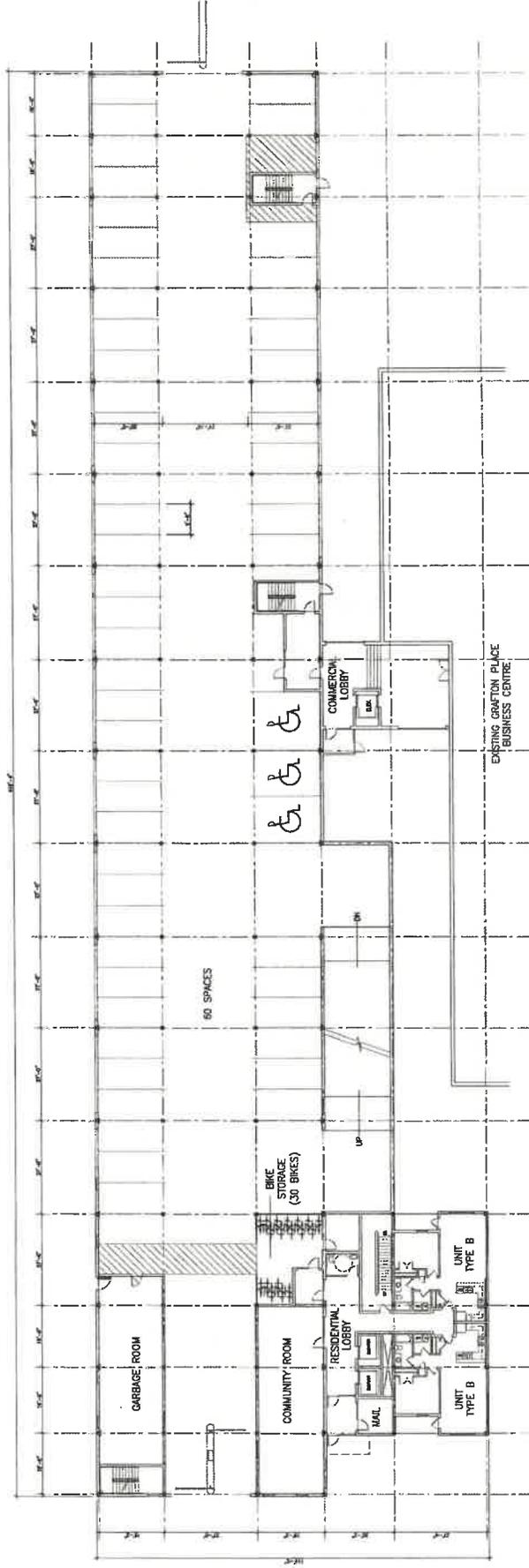
LOWER LEVEL - NOT TO SCALE

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A-0

PORT HOUSE APARTMENTS
 Charlottetown, PE - February 17, 2021 (Rev.#1)



PARKING GARAGE AREA 29,855 SQ.FT.
 LIVING AREA 3,000 SQ.FT.
 (2-UNITS)

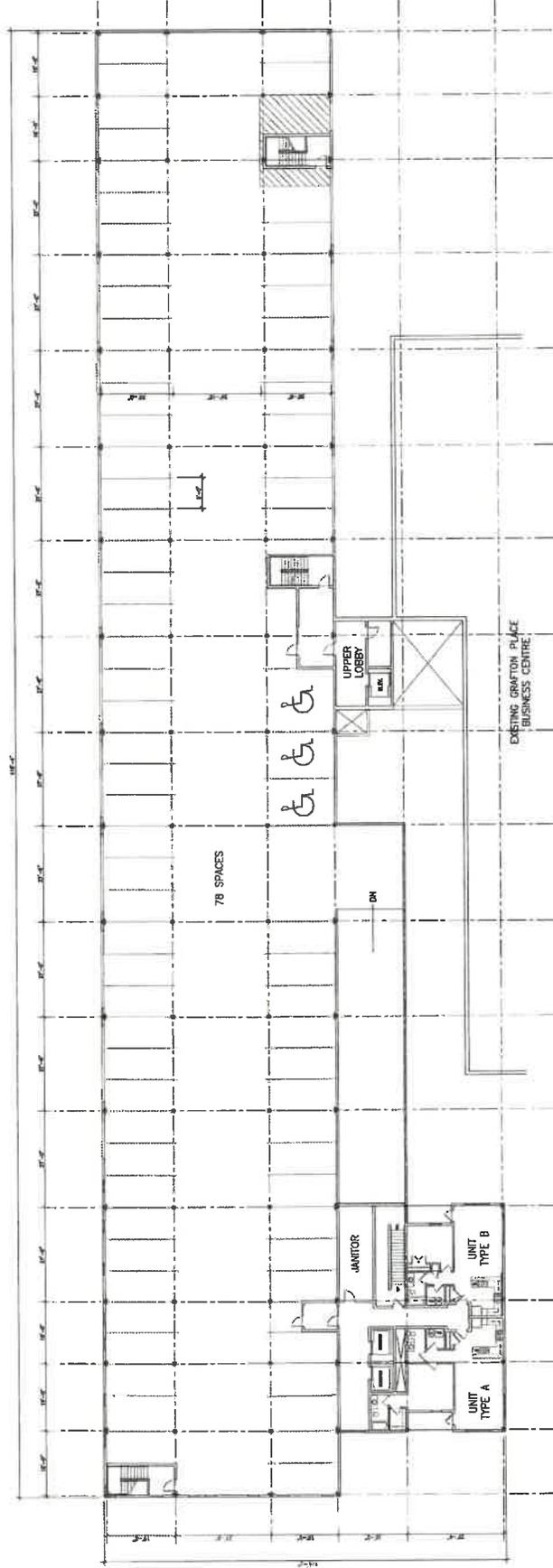
MAIN LEVEL - NOT TO SCALE

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A-1

PORT HOUSE APARTMENTS
 Charlotte, NC - February 17, 2021 (Rev.#1)



PARKING GARAGE AREA 29,855 SQ.FT.
 LIVING AREA 3,000 SQ.FT.
 (2-UNITS)

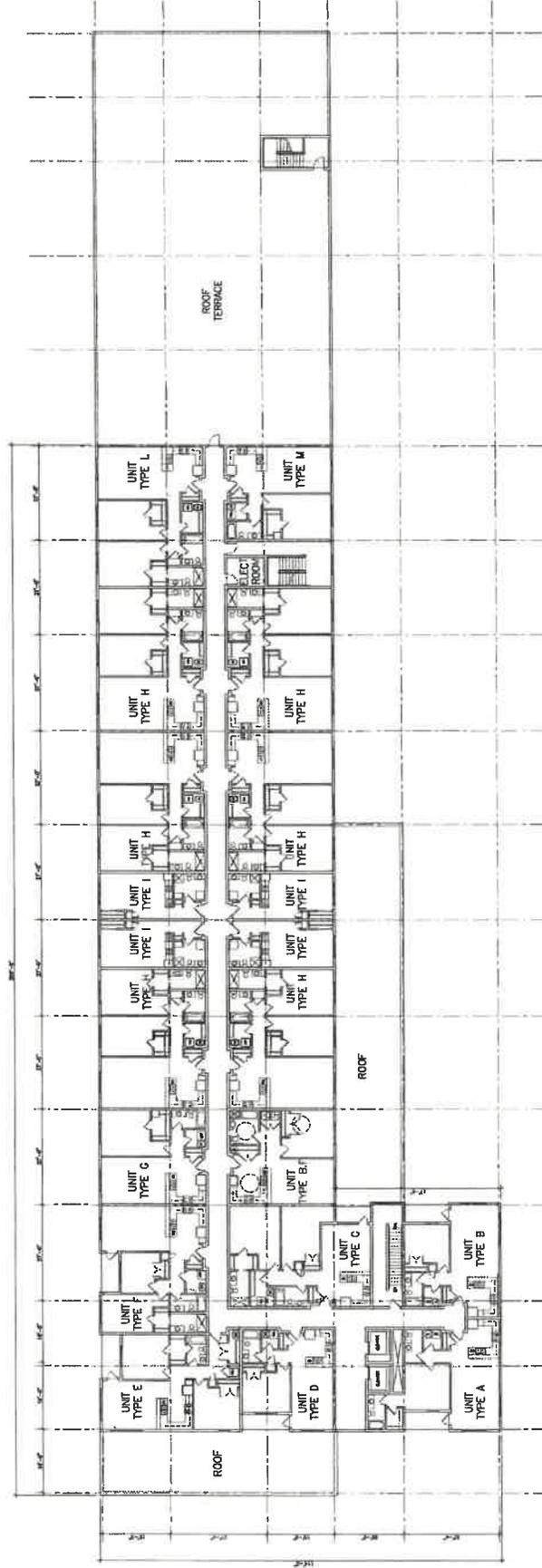
SECOND LEVEL - NOT TO SCALE

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A-2

PORT HOUSE APARTMENTS
 Charlotte town, PE - February 17, 2021 (Rev.#1)



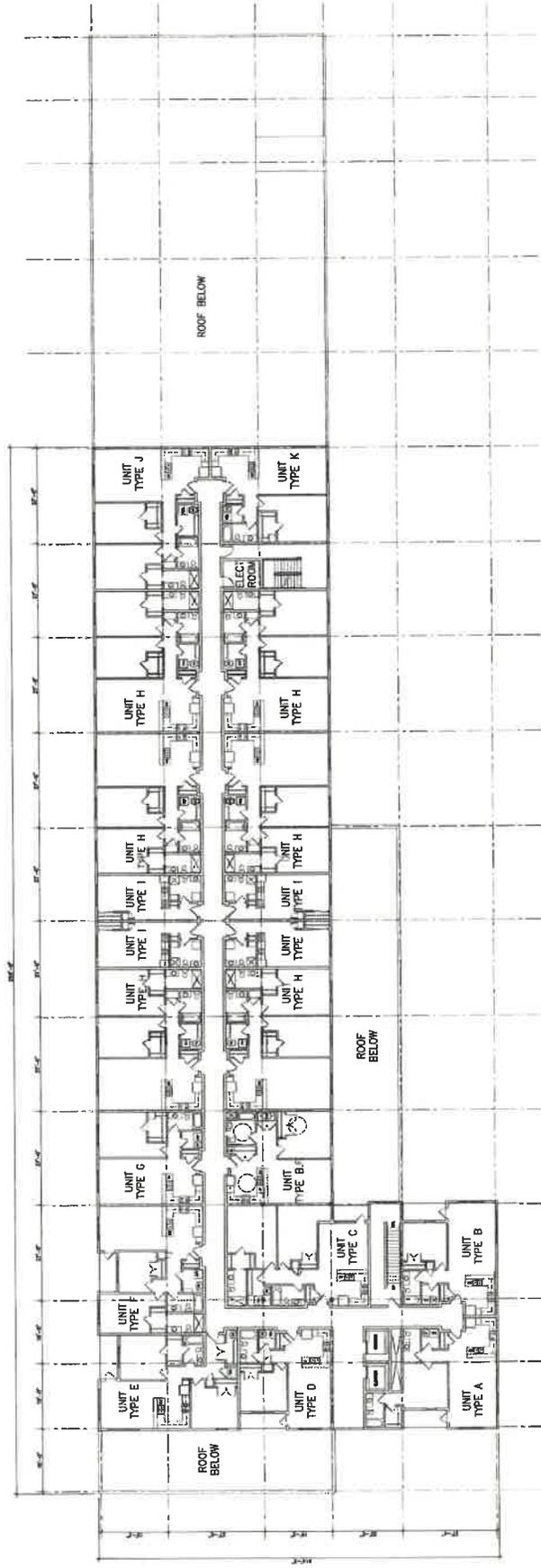
LIVING AREA
(20-UNITS)

21,467 SQ.FT.

THIRD LEVEL - NOT TO SCALE

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21,467 SQ.FT. (PER/FLOOR)

LIVING AREA
(20-UNITS PER/FLOOR)

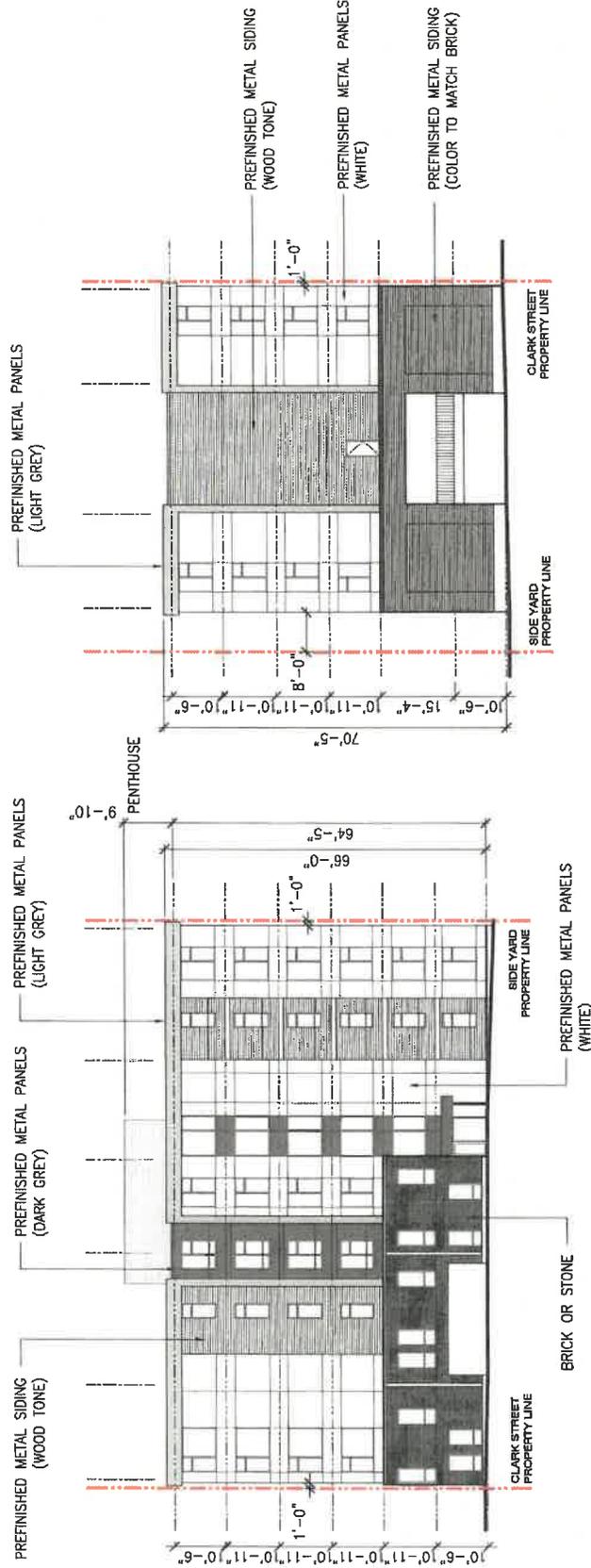
A-4

LEVELS 4-6 - NOT TO SCALE

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PORT HOUSE APARTMENTS
Charlottetown, PE - February 17, 2021 (Rev.#1)





FACING HILLSBOROUGH STREET

FACING PRINCE STREET

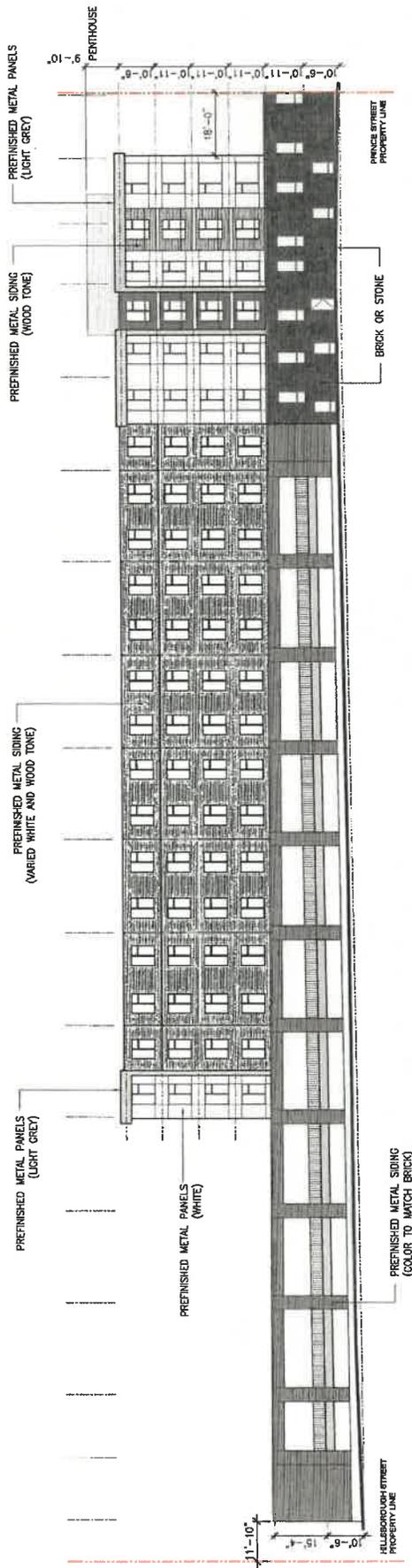
FRONT AND REAR ELEVATIONS - NOT TO SCALE

A-5

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PORT HOUSE APARTMENTS
 Charlottetown, PE - February 17, 2021 (Rev.#1)



FACING CLARK STREET

LEFT LEVEL - NOT TO SCALE

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A-7

POPE HOUSE APARTMENTS
 Charlotte, PE - February 17, 2021 (Rev.#1)

Attachment D

Rendered Views

<p>SITE SPECIFIC EXEMPTION File: PLAN-2021-06-B-5 199 GRAFTON STREET (PID# 342790)</p> <p>OWNER: 102390 PEI Inc.</p> <p>APPLICANT: APM Commercial Royal LePage</p>	 <p>CHARLOTTETOWN Planning & Heritage Department</p>
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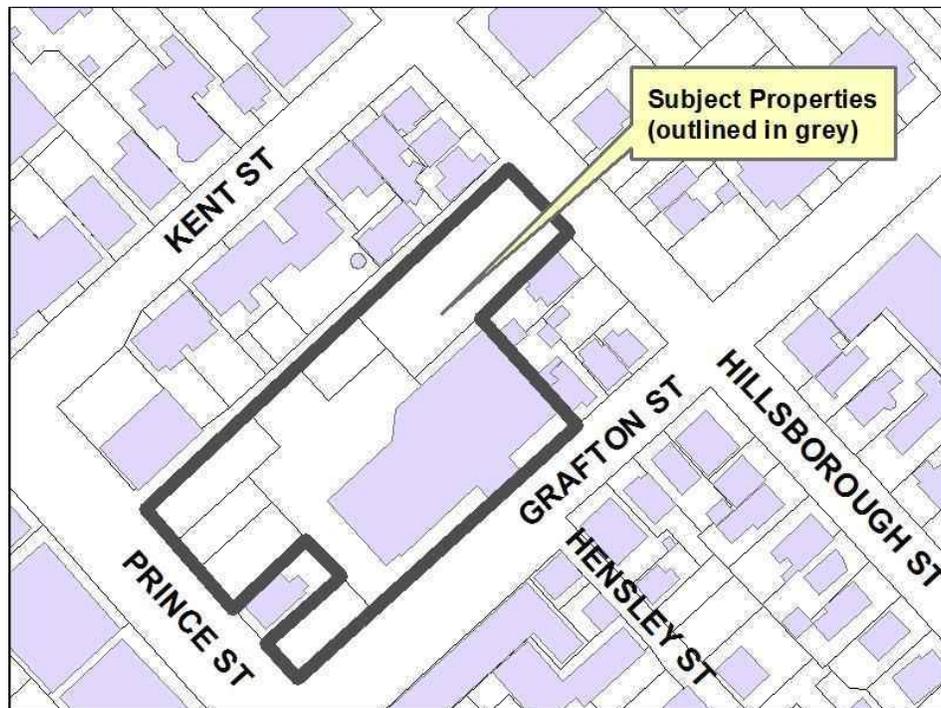






Attachment D

Map Showing Location of Property



File: DESIGN-2023-30-OCTOBER 6B
199 GRAFTON STREET (PID# 342790)

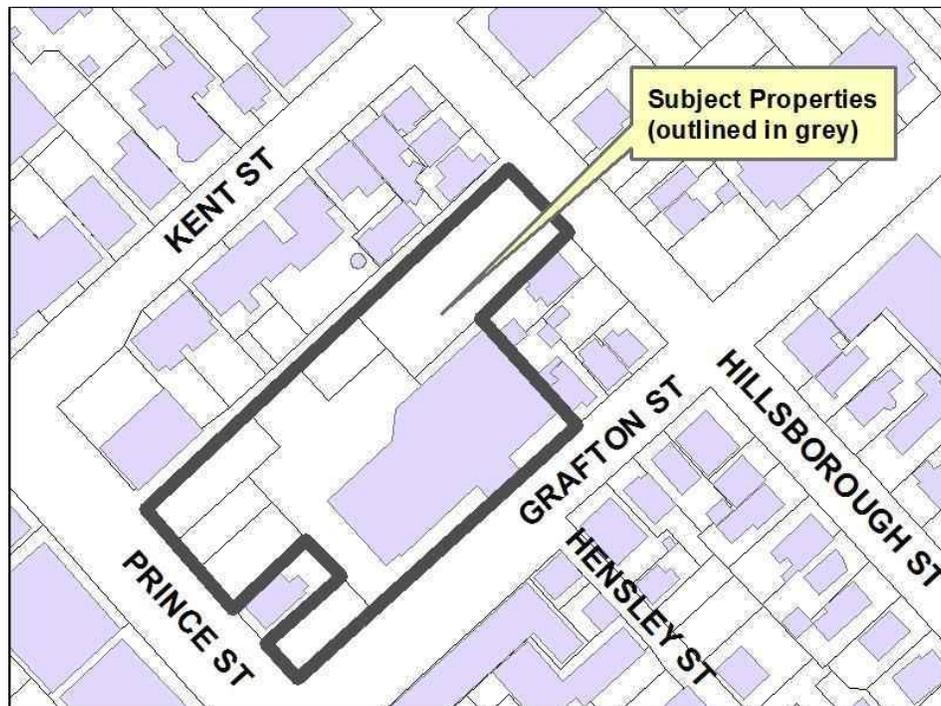
OWNER: 102390 PEI Inc.

(c/o Tim Banks, APM Commercial Royal LePage)


CHARLOTTETOWN
Planning & Heritage
Department

Attachment D

Map Showing Location of Property



File: DESIGN-2023-30-OCTOBER 6B
199 GRAFTON STREET (PID# 342790)

OWNER: 102390 PEI Inc.

(c/o Tim Banks, APM Commercial Royal LePage)



TITLE: <p style="text-align: center;">DESIGN REVIEW FILE: DESIGN-2023-30-OCTOBER-6C 40 RIVERSIDE DRIVE (PID #839332) OWNER: THUNDERBOLT ENERGY ASSETS GP INC. APPLICANT: NAZMI LAWEN</p>		 CHARLOTTETOWN
MEETING DATE: October 30, 2023		Page 1 of 5
DEPARTMENT: Planning & Heritage	ATTACHMENTS: <ul style="list-style-type: none"> A. Design Review Submission (May 5, 2023) B. External Design Reviewer's Comments (June 8, 2023) C. Updated Site Plan and 3D Concepts (July 27, 2023) D. External Design Reviewer's Comments (July 31, 2023) E. Updated Site Plan (August 16, 2023) F. Response from Enwave (July 27- August 24, 2023) G. External Design Reviewer's Comments (August 25, 2023) H. GIS Map Showing Location 	
SITE INFORMATION: Context: approximately 1.37 ha (3.38 acre) property on the south side of Riverside Drive, abutting the Hillsborough River, approximately 400 metres (1,300 feet) east of the Eastern Gateway Waterfront Plan Study Area. Ward No: 2 – Belvedere Existing Land Use: Waste to energy facility Official Plan: Industrial Zoning: Light Industrial Zone (M-1)		

RECOMMENDATION:

The Planning & Heritage Department encourages the Design Review Board to conditionally approve the site and building plans for the proposed waste to energy building to be constructed on 40 Riverside Drive (PID #839332) as per the Design Reviewer's report.

BACKGROUND:

Request

The applicant (Nazmi Lawen) on behalf of the property owner (Thunderbolt Energy Assets GP Inc.) has submitted a design for a proposed new waste to energy building on the subject property including:

- An increase in the maximum permitted height from 12 metres to approximately 35 metres for a building in the M-1 Zone;

- An increase in the maximum allowable height of rooftop mechanical equipment from 3 metres to 15 metres above the maximum permitted height;
- A reduction in the minimum required setback of the rooftop mechanical equipment from 3 metres to 2 metres from the roof edge.

According to the applicant, the new waste to energy building is intended to increase the waste feed throughput, reduce facility maintenance requirements, improve the system reliability and reduce fossil fuel emissions. The new waste to energy facility will replace the existing waste incinerators on the subject property.

The request is to approve the design proposal.

By-law Requirement.

Even though this project would not require design review, the site-specific exemption process provided discretion to Council to place conditions on an approval, a condition of the proposal to undergo design review would be appropriate to address the design of the structure and possible site improvements regarding landscaping, shoreline access and naturalization measures.

ANALYSIS:

On May 5, 2023, the applicant submitted conceptual drawings (site plan, floor plans, elevations, renderings, and ramp Information) please refer to Attachment A, of the proposed new waste to energy building. Staff sent the proposed design package to an independent design reviewer, Robert Matthews, BArch, AAPEI to perform a design review.

On June 8, 2023, Mr. Matthews submitted his formal review, as per Attachment B, which provided the following feedback and guidance:

The site is an important one. It is highly visible from the Route 1 causeway as well from Stratford and therefore, requires careful design development to enhance this location.

The suggested massing and detail suggest a contemporary solution to an industrial building. The linear windows on the upper level of the South façade are nice and will add visual interest to the façade both during the day and night. The exterior appears to be a clean minimalist aesthetic. If it evolves to become the preferred approach, it shouldn't become cluttered with "engineering" grilles, vents, pipes, etc. There is an office component on piloti which I think may be an overhang on a drive aisle. The size of the windows and placement is nice, and I believe as the design evolves can play a significant role in the façade of this element.

I'm having difficulty in determining the staff/main entrance to the building. There is a long "diagonal" stair leading from the ground to the Level 3. If the diagonal stair is simply an exit, then consideration should be given to a more compact shape and perhaps combined with the more conventional internal exit office stair. If this is not possible, then consideration should be given to a more graceful structure. The elevator is tucked further into the footprint and adjacent to a lobby with a cross corridor. Emphases need to be given to this south "main" entrance. I realize it's internal to the site [behind the existing building], but it should be made visually more interesting.

As mentioned above, the site is quite important as it has exposure from three directions: east and west on Highway 1 and from the south across the water. From these views, attention needs to be made to how the building will read from these perspectives.

There can be several design vocabularies.

One being a very elegant building [as presented] with subtle use of color or colour shades to accentuate the mass and volumes. This should also address the base of the facility.

Another scenario is to look at all facades as a canvas and for the proponent to create a visually stimulating façade. A couple of examples might be: -

- in the cladding, incorporate a geometric pattern - Mondrian, or Francois Morellet as inspiration. Reference the graphics on the fly tower of the Fredericton Playhouse,*
- projected images, either as still life or a video projection, such as in Quebec City where there is a long bank of grain silos that are illuminated at night with colour changing LED lights.*

Please keep in mind that all of the building elevations are equal, highly visible, and therefore similar consideration should be given to their treatment. In any scenario thought should be given to the size, placement and geometry of the fenestration, overhead doors, and other means of access.

Finally, the site needs to be addressed in a thoughtful way. In looking at google earth the site needs organization to reduce the amount of asphalt, attention to the need and placement of the miscellaneous small structures and general onsite storage and screening of equipment. The site plan, as presented suggests simply adding the building and extending the property into the basin.

The city's official plan addresses a pedestrian link that would extend from along the west waterfront across this site and over to the Queen Elizabeth Hospital property. Now is the time to allow for this pathway.

With the encroachment of the site into the waterfront, the shoreline has been reinforced with rip rap. The importance of this is not lost, but a more environmentally sound solution needs to be incorporated by the greening of the design. With the relocation of the hot water silo, there is an opportunity to reduce the heat island effect of the yard whilst maintaining the truck movements and separating the small vehicle parking, from the heavy vehicles. The rationalized space would be landscaped which would help address the view of the existing facility as seen from the street and pedestrian pathway.

The proponent should demonstrate the vehicle movements to help determine how the site is used. There are creative solutions to storm water management which could be considered. The bonus would be the greening of the site which builds on the environmental "spin" of the facility, whilst improving the aesthetics as seen from the causeway, Stratford and providing a buffer to the proposed pedestrian pathway.

Overall, the project has nice massing, but the next iteration needs to take the design to the next level where more of the building design and urban character needs to be explored.

On July 27, 2023, the applicant submitted revised conceptual drawings, as per Attachment C, which included plans and renders addressing the reviewer's comments.

In response, the design reviewer submitted a follow-up letter on July 31, 2023, as per Attachment D, which identified the following matters that were not addressed in the revised drawings:

- 1. Reduce the amount of asphalt** – *The current design has not addressed this. The truck movements indicate that the asphalt could be reduced extensively.*
- 2. Miscellaneous small structures and on-site storage** - *Street view showed small structures that are unsightly. The final approved site plan should address on site storage. If it cannot be accommodated in the new building, then a more permanent solution needs to be considered – either a small structure or a designed screened enclosure.*
- 3. Hot water Tank** – *the site does not show the location of the "relocated hot water tank." This will affect the site redevelopment.*

4. **Heat island effect** – *in order to reduce this and to increase the landscaped area, the hot water tank location needs to be sited and the truck and car movements better planned.*

In response to the follow-up letter, a revised plan and set of notes were supplied by the applicant, as per Attachment E and F.

On August 25, 2023, the design reviewer submitted the final review letter, as per Attachment G, which included the following feedback:

I have reviewed the above submission and appreciate the situation with respect to land ownership and the unknown future development plans. With this in mind, I find the proposed conceptual landscaping to be acceptable. I would recommend that the City work with the applicant's landscape architect to ensure that the areas highlighted in green are treated in an appropriate manner.

Overall, the design reviewer supported the project and determined that the proposal is conditionally acceptable with a note that landscape detail should be provided during the next Application process.

Planning Staff agree with the design reviewer that the concerns raised support a number of key points from the Official Plan for consideration including:

*Section 3.8 (Establishing a Sustainable Community) – Our **objective** is to work with the province to review, identify, and protect all natural areas and wildlife habitats considered to be of aesthetic, biological, ecological, or geological importance for the benefit and enjoyment of present and future generations.*

*Section 3.8 (Establishing a Sustainable Community) – Our **policy** shall be to require that any proposed development adjacent to a designated Natural Area should respect and endeavour to incorporate natural elements into its design and construction, and to maintain the property in a manner sympathetic to its environment.*

*Section 3.8 (Establishing a Sustainable Community) – Our **objective** is to ensure that Charlottetown's air quality is not endangered by actions taken by the public or private sectors.*

*Section 4.8 (Appropriate Industrial Development) - Our **objective** is to ensure that industrial zones do not detract from Charlottetown’s character and appeal.*

*Section 4.8 (Appropriate Industrial Development) - Our **policy** shall be to work with property owners in established industrial zones to minimize unsightliness and to create a uniform standard for the appearance and upkeep of properties.*

*Section 4.8 (Appropriate Industrial Development) - Our **policy** shall be to minimize the land-use conflicts which might exist or arise between existing industrial zones and their non-industrial neighbours.*

Therefore, on this basis, Planning staff recommend conditional approval of the proposal addressing the concerns raised by the design reviewer. To reiterate, a landscape design submission is required to address the below mentioned points during the building permit process:

1. Clouded areas marked in green (Attachment E) need to incorporate trees, shrubs, and planting locations.
2. Detailed solution required particularly at the corner where the tank is currently located (Attachment E). With the relocation of the hot water silo, there is an opportunity to incorporate landscaping that can delineate where the parking as well as drive aisles for small vehicles are located. Drive lanes for heavy vehicles should be clearly marked and small vehicle parking located away from heavy vehicles.
3. Detailed solution required at the area marked as “Greenspace opportunity TBD” (Attachment E) alongside of the vehicular entrance. This greenspace can act as screening/landscaping enhancement for the parking area.
4. A sound vehicular movement plan should be incorporated with the landscaping design drawings that shall include but not be limited to:
 - Surface treatment of all soft and hard surfaced landscaped areas.
 - Planting schedule (e.g. name, height, diameters of canopy) with quantity.
5. There is a need to reduce asphalt surface coverage as well as provide screening, which can be achieved with the landscaping design.
6. In fulfillment of conditions 1 to 5 (inclusive), it is required that the applicant provide appropriate drawings to satisfy these conditions that have been created and stamped by a professional Landscape Architect who is licensed to practice in the Province of Prince Edward Island.

CONCLUSION:

Planning staff recommend that the Design Review Board approve the proposed building design subject to the noted conditions for the proposed structure at 40 Riverside Drive (PID #839332).

PRESENTER:



Sonia Kamal, MPlan, BArch
Planner II- Development

MANAGER:



David Douglas Gundrum, RPP, MCIP
Manager of Development Planning

Attachment A

Design Review Submission May 5, 2023

File: DESIGN-2023-30-OCTOBER-6C
40 Riverside Drive (PID #839332)
Applicant: Nazmi Lawen
Owner: Thunderbolt Energy Assets GP Inc



PEI Energy Systems Expansion

PEI Energy Systems
 40 Riverside Drive
 Charlottetown, PE

ILLUSTRATIVE DESIGN ONLY NOT FOR CONSTRUCTION

CIVIL

- C100 Existing Site Plan & Demolition
- C101 New Site Plan
- C102 Embankment Extension Profiles

ARCHITECTURAL

- A001 Context Plan & Project Info
- A030 3D Views
- A031 3D Views
- A100 Overall Plan - Levels 1,2 & 3
- A101 Overall Plan - Level 4 & Roof Plan
- A102 Enlarged Plan - Level 1
- A103 Enlarged Plan - Level 2
- A104 Enlarged Plan - Level 3
- A105 Enlarged Plan - Level 4
- A140 Floor Finish Plan - Levels 1 & 2
- A141 Floor Finish Plan - Levels 3 & 4
- A160 Reflected Ceiling Plan - Levels 1 & 2
- A161 Reflected Ceiling Plan - Levels 3 & 4
- A200 Exterior Elevations
- A300 Building Sections
- A301 Building Sections
- A302 Building Sections

MECHANICAL

- M100 Overall Plan - Levels 1, 2 & 3 Plumbing
- M101 Overall Plan - Level 4 Plumbing & Details
- M200 Overall Plan - Levels 1, 2 & 3 Hydronics
- M201 Overall Plan - Level 4 Hydronics & Details
- M202 Overall Plan - Hydronic Details
- M300 Overall Plan - Levels 1, 2 & 3 Ventilation
- M301 Overall Plan - Level 4 Ventilation
- M302 Overall Plan - Levels 1, 2 & 3 Ventilation
- M303 Overall Plan - Level 4 Ventilation
- M304 Overall Plan - Levels 1, 2 & 3 Ventilation
- M305 Overall Plan - Levels 4 & 5 Ventilation
- M306 Overall Plan - Ventilation Details

ELECTRICAL

- E100 Site Plan & Details
- E200 Level 1, Level 2, Level 3 & Level 4 - Power
- E300 Level 1, Level 2, Level 3 & Level 4 - Lighting
- E400 Level 1, Level 2, Level 3 & Level 4 - Communications & Systems
- E500 Details
- E501 Details
- E502 Details



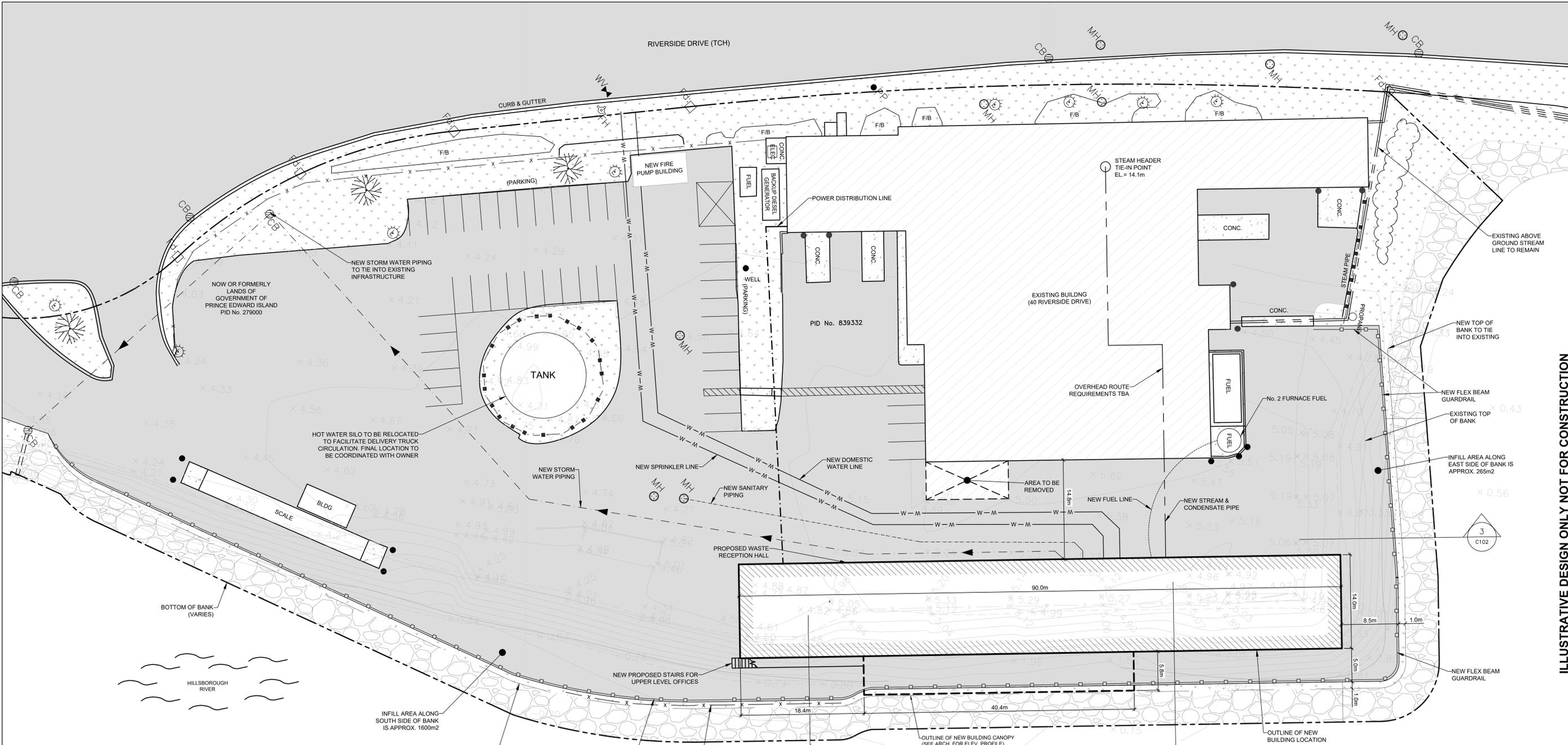
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Client
 PEI Energy Systems

Project Title
 PEI Energy Systems Expansion
 40 Riverside Drive
 Charlottetown, PE

Sheet Title
 Cover Page

No.	Description	Date	Date:	Revision
0	Issued for RFP	2022-11-18	2022-11-18	0
			Dim By: JLA	
			Chk By: SDM	
			Project Number:	
			221079	
			Drawing Number:	
			A000	



1 SITE PLAN: NEW WORKS
C101 1:250

LEGEND

	PROPERTY LINE
	EXISTING CONTOUR
	EXISTING BUILDING
	NEW BUILDING
	EXISTING EDGE OF ASPHALT
	NEW EDGE OF ASPHALT
	EXISTING/NEW CONCRETE CURB
	EXISTING SANITARY SEWER MAIN
	NEW SANITARY SEWER SERVICE
	EXISTING WATER MAIN
	NEW WATER SERVICE
	NEW CHAIN LINK FENCE
	NEW GUARD RAIL
	EXISTING UTILITY POLE
	STORM CATCH BASIN
	SANITARY MANHOLE
	FIRE HYDRANT
	WATER VALVE
	CURB STOP
	BOREHOLE LOCATION
	EXISTING LIGHT FIXTURE

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				0	Issued for RFP	2022-12-06	2022-12-06	0
				<p>Project Number: 221079</p>		<p>Date: 2022-12-06 Dm By: M.K. E.I.T. / SB Chk By: N.L. P.Eng</p>		
				<p>Drawing Number: C101</p>				

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Context Site Plan
1 : 2000

- GENERAL NOTES**
1. VERIFY ALL DIMENSIONS, THESE ARE ILLUSTRATIVE DRAWINGS ONLY.
 2. THE WORK SHALL BE IN ACCORDANCE WITH THE NATIONAL BUILDING CODE OF CANADA (NBCC 2015).
 3. COMPLY WITH ALL LOCAL MUNICIPAL AND PROVINCIAL BY-LAWS AND REGULATIONS.
 4. DESIGN BUILDER TO VERIFY LOCATION OF ALL UTILITIES PRIOR TO EXCAVATION.
 5. BENCH MARKS TO BE ESTABLISHED BY DESIGN BUILDER.
 6. ALL OTHER DISCIPLINES SHOWN FOR GENERAL INTENT ONLY. REFER TO CORRESPONDING DRAWINGS.
 7. REFER TO PERFORMANCE SPECIFICATION BOUND SEPARATELY.
 8. DESIGN BUILDER TO DETERMINE ALL MINIMUM FIRE RESISTANCE RATINGS FOR FIRE SEPARATIONS OF THE FLOORS, WALLS AND ROOFS AS REQUIRED.
 9. DESIGN BUILDER TO DETERMINE MINIMUM STC AND IIC OF ALL ASSEMBLIES.
 10. DESIGN BUILDER TO DETERMINE FINAL OCCUPANT LOAD AND NUMBER OF WASHROOM FIXTURES REQUIRED.
 11. DESIGN BUILDER TO PROVIDE ALL NECESSARY WALKWAYS AND STAIRS AROUND ALL EQUIPMENT TO ALLOW FOR OWNER OPERATION, MAINTENANCE AND EGRESS.

FLOOR PLAN LEGEND

	ROOM NAME
	WALL TAG

PROJECT INFORMATION

ADDRESS:	40 RIVERSIDE DRIVE
MUNICIPALITY:	CHARLOTTETOWN
PID#:	839332
ZONING:	M1 - LIGHT INDUSTRIAL
SETBACKS:	
LOT FRONTAGE:	20.0m
FRONT YARD:	7.5m
REAR YARD:	7.5m
SIDE YARD:	3.0m
FLANKAGE YARD:	7.5m
MAX. HEIGHT:	12.0m
GROSS AREAS:	
LEVEL 1:	1242m ²
LEVEL 2:	313m ²
LEVEL 3:	526m ²

EXTERIOR WALL TYPES

TYPE	DESCRIPTION	FRR	STC	IIC	REMARKS
EW1	- 4"/100mm INSULATED METAL PANELS, VERTICAL (MIN. R27) - HORIZONTAL METAL GIRTS (DESIGN BUILDER TO DETERMINE SPACING AND DEPTH) - STEEL (DESIGN BUILDER TO DETERMINE THICKNESS AND DEPTH)				
EW2	- 4"/100mm INSULATED METAL PANELS, VERTICAL (MIN. R27) - HORIZONTAL METAL GIRTS (DESIGN BUILDER TO DETERMINE SPACING AND DEPTH) - CONCRETE (DESIGN BUILDER TO DETERMINE THICKNESS AND DEPTH)				

FOUNDATION WALL TYPES

TYPE	DESIGN NO.	DESCRIPTION	FRR	STC	IIC
FW1		- CONTINUOUS WATERPROOF MEMBRANE - 2"/50mm RIGID INSULATION (R10) - CONCRETE WALL (DESIGN BUILDER TO DETERMINE THICKNESS) - WALL FINISH AS SCHEDULED			
FW2		- 10mil SLIP SHEET (TAPED AND SEALED, EXTEND FROM TOP OF FOOTING TO FINISH GRADE) - 2"/50mm RIGID INSULATION (R10) EXTEND FROM TOP OF FOOTING TO 6"/150mm BELOW FINISH GRADE - CONCRETE (DESIGN BUILDER TO DETERMINE THICKNESS)			

PARAPET WALL TYPES

TYPE	DESIGN NO.	DESCRIPTION	FRR	STC	IIC
PW1		- 4"/100mm INSULATED METAL PANELS, VERTICAL (MIN. R27) - 3-5/8"/92mm METAL STUD FRAMING @ 16"/400mm O.C. C/W BATT INSULATION (R24) - 5/8"/16mm FIBREGLASS MAT GYPSUM SHEATHING - CONTINUOUS AIR BARRIER (SELF ADHESIVE) - 1/4"/6mm PROTECTION BOARD - 2 PLY SBS MEMBRANE (LIGHT GRAY)			

CONCRETE WALL TYPES

TYPE	DESIGN NO.	DESCRIPTION	FRR	STC	IIC
C1		- CONCRETE (DESIGN BUILDER TO DETERMINE THICKNESS)			
C2		- CONCRETE OR CONCRETE BLOCK (DESIGN BUILDER TO DETERMINE THICKNESS) - 3-5/8"/92mm METAL STUD FRAMING @ 16"/400mm O.C. C/W SPRAY FOAM INSULATION (FILL CAVITY) - 5/8"/16mm GYPSUM WALL BOARD			

BLOCK WALL TYPES

TYPE	DESIGN NO.	DESCRIPTION	FRR	STC	IIC
B1		- 7.5"/190mm CONCRETE BLOCK			

METAL STUD WALL TYPES

TYPE	DESIGN NO.	DESCRIPTION	FRR	STC	IIC
M1		- 5/8"/16mm GYPSUM WALL BOARD - 3-5/8"/92mm METAL STUD FRAMING @ 16"/400mm O.C. C/W MINERAL WOOL INSULATION (FILL CAVITY) - 5/8"/16mm GYPSUM WALL BOARD			
M2		- 5/8"/16mm GYPSUM WALL BOARD - 6"/152mm METAL STUD FRAMING @ 16"/400mm O.C. C/W MINERAL WOOL INSULATION (FILL CAVITY) - 5/8"/16mm GYPSUM WALL BOARD			

GLAZING WALL TYPES

TYPE	DESIGN NO.	DESCRIPTION	FRR	STC	IIC
G1		- 63.5mm SIGHTLINE x 160.3mm OVERALL DEPTH, 101.6mm BACK MULLION - TRIPLE GLAZED SEALED UNIT - REFER TO ELEVATIONS FOR HORIZONTAL MULLIONS			

FLOOR TYPES

TYPE	DESCRIPTION	REMARKS
F1	- FLOOR FINISH AS SCHEDULED - CONCRETE FLOOR (DESIGN BUILDER TO DETERMINE THICKNESS) - CONTINUOUS WATERPROOF MEMBRANE - 2"/50mm RIGID INSULATION (R10) - COMPACTED MATERIAL (AS PER GEOTECHNICAL REQUIREMENTS)	
F2	- FLOOR FINISH AS SCHEDULED - CONCRETE FLOOR (DESIGN BUILDER TO DETERMINE THICKNESS) - 10MIL POLY VAPOUR BARRIER (TAPED AND SEALED) - 2"/50mm RIGID INSULATION (R10) - COMPACTED MATERIAL (AS PER GEOTECHNICAL REQUIREMENTS)	
F3	- FLOOR FINISH AS SCHEDULED - CONCRETE (DESIGN BUILDER TO DETERMINE THICKNESS) - METAL DECKING (DESIGN BUILDER TO DETERMINE THICKNESS) - STEEL STRUCTURE (DESIGN BUILDER TO DETERMINE SPACING AND DEPTH) - SPRAY FIRE PROOFING (DESIGN BUILDER TO DETERMINE REQUIREMENTS) - REFER TO RCP FOR CEILING FINISH	

ROOF TYPES

TYPE	DESCRIPTION	REMARKS
R1	- 2 PLY SBS MEMBRANE (LIGHT GRAY) - 1/4"/6mm PROTECTION BOARD - 7"/178mm RIGID INSULATION (R40), MINIMUM (R36.4) PER NECB 2017 - VAPOUR BARRIER (SELF ADHESIVE) - 5/8"/16mm FIBREGLASS MAT GYPSUM SHEATHING - METAL DECKING (DESIGN BUILDER TO DETERMINE THICKNESS), REFER TO RCP FOR PERFORATED LOCATIONS - STEEL STRUCTURE (DESIGN BUILDER TO DETERMINE SPACING AND DEPTH) - REFER TO ROOF PLAN FOR SLOPES	

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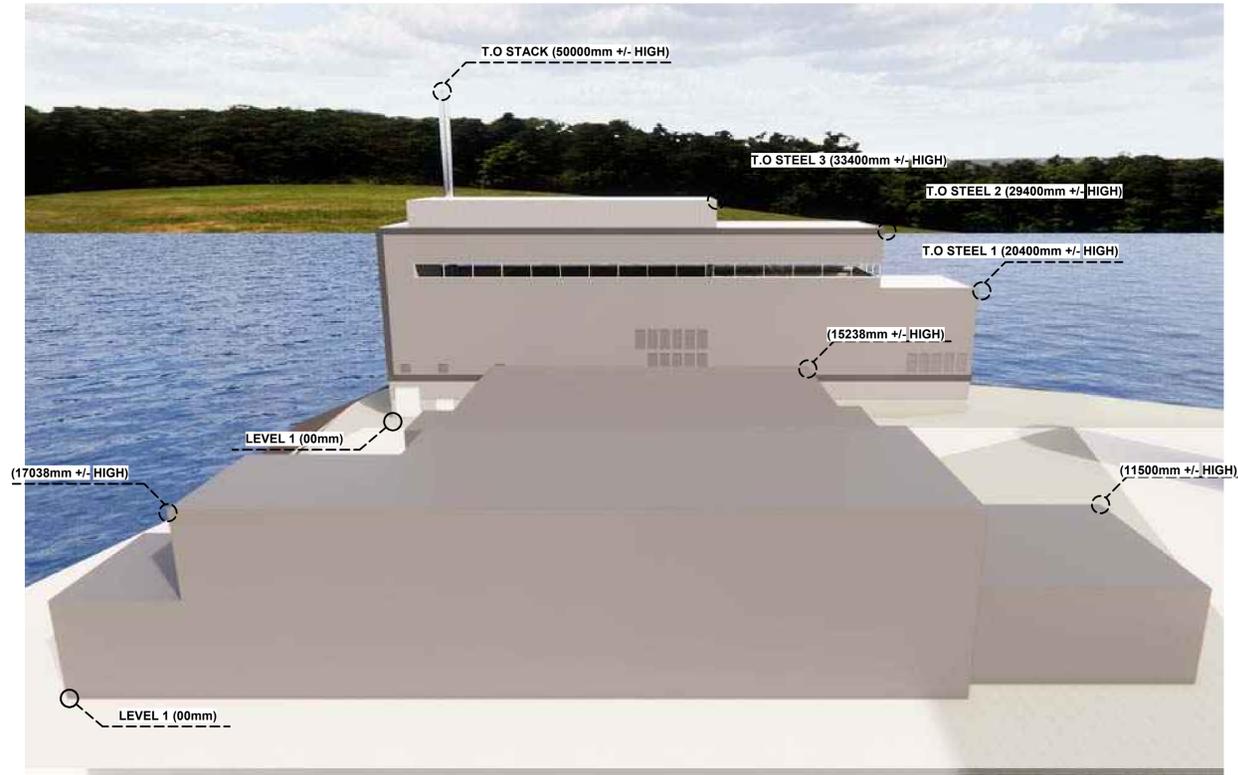
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Client
PEI Energy Systems

Project Title
PEI Energy Systems Expansion
40 Riverside Drive
Charlottetown, PE

Sheet Title
Context Plan & Project Info

No.	Description	Date	Date: 2022-11-18	Revision
0	Issued for RFP	2022-11-18	Drn By: JLA	0
			Chk By: SDM	
			Project Number:	
			221079	
			Drawing Number:	
			A001	



VIEW FROM NORTH-WEST

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REFERENCE BUILDINGS IN CITY	
NAME	HEIGHT(mm)
UPEI CANADA GAMES RESIDENCE	35400
DELTA	36600
HOLMAN GRAND	38700
ST. DUNSTAN'S BASILICA	50000

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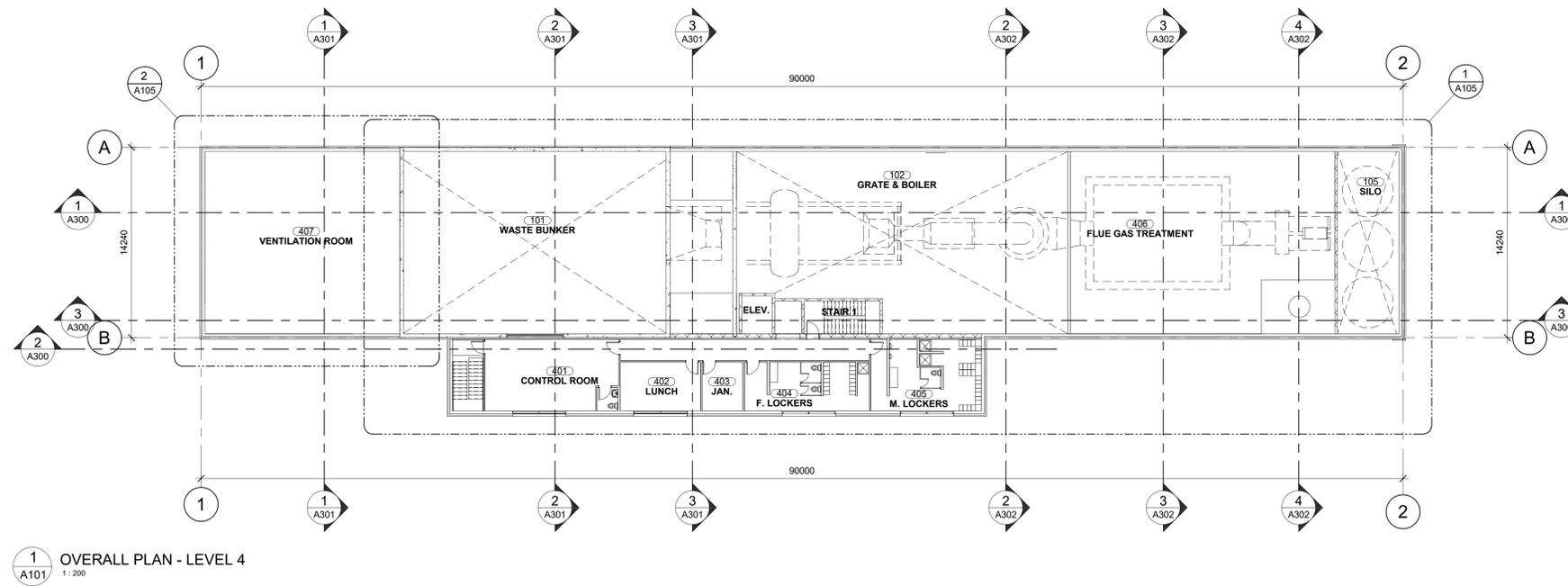
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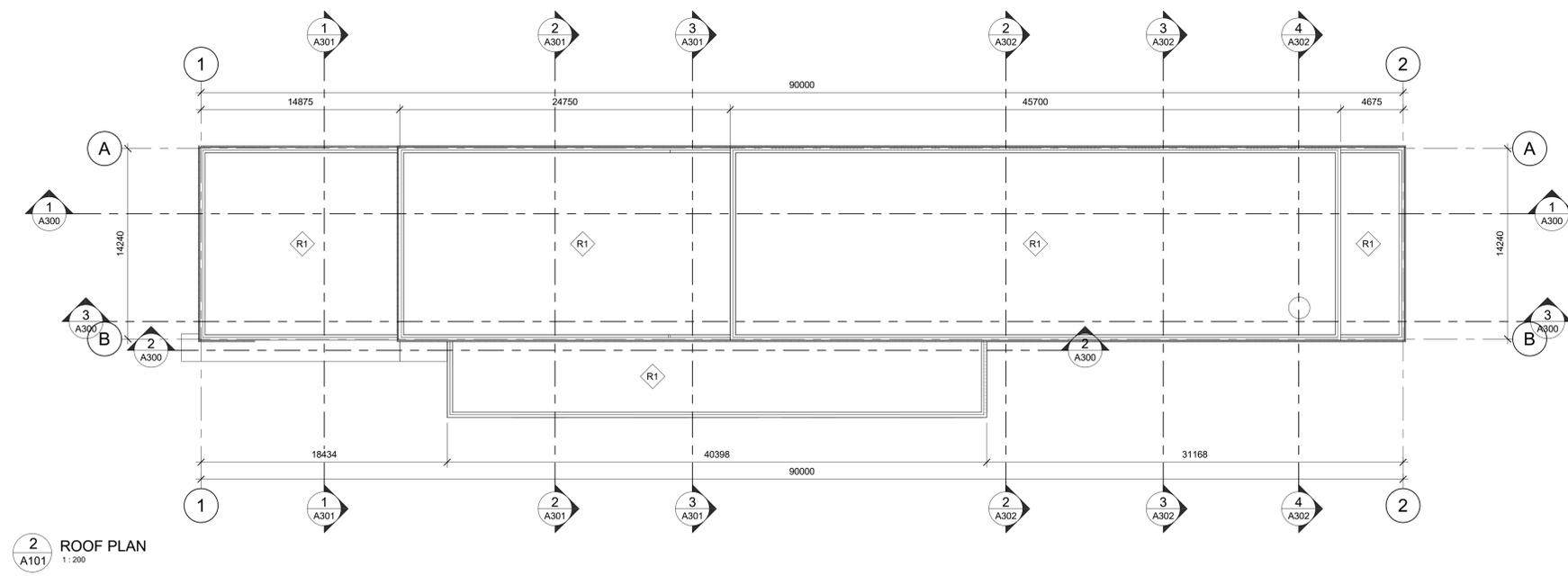
Project Title
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40 Riverside Drive
Charlottetown, PE

Sheet Title
3D View

No.	Description	Date	Date:	Revision
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			Dim By: GR	
			Chk By: SDM	
			Project Number:	
			221079	
			Drawing Number:	
			A031	



1 OVERALL PLAN - LEVEL 4
A101 1:200



2 ROOF PLAN
A101 1:200



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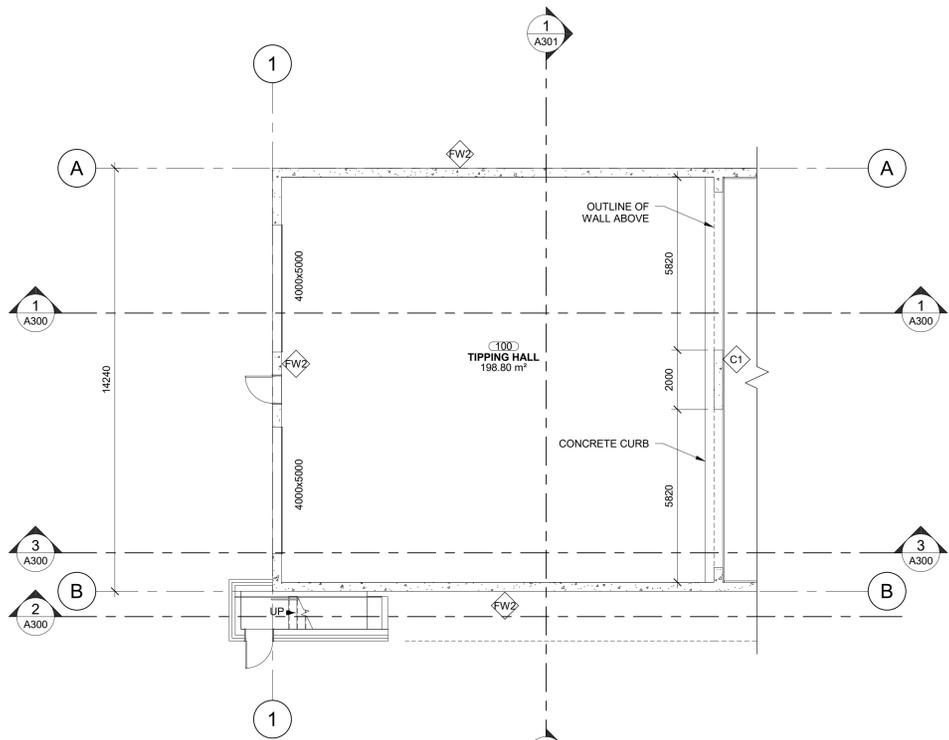
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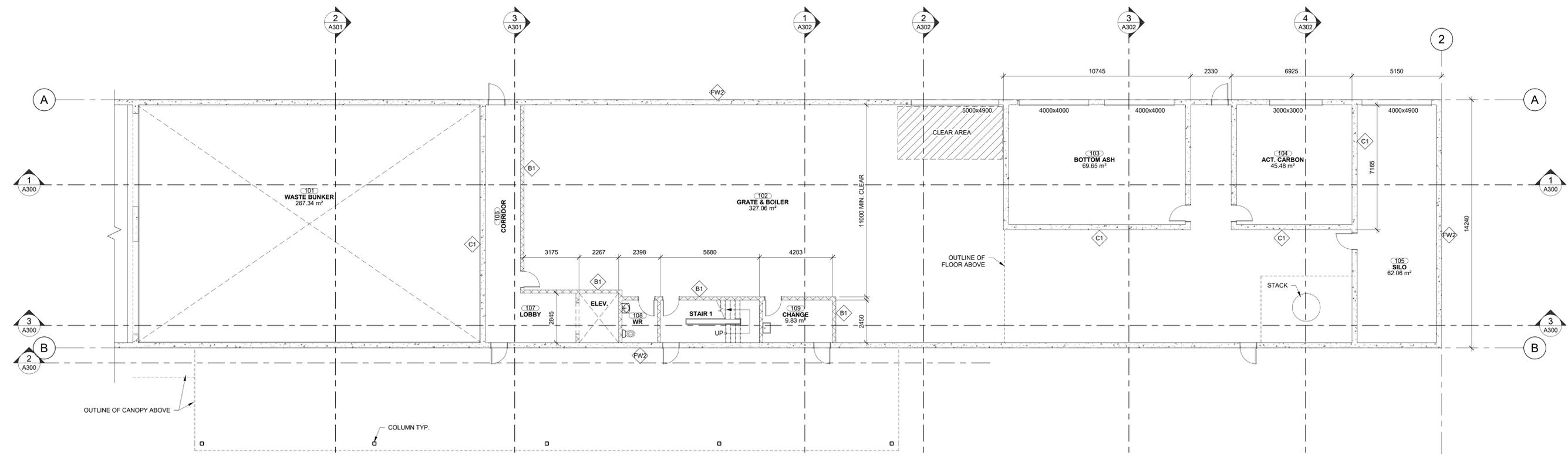
Project Title
PEI Energy Systems Expansion
40 Riverside Drive
Charlottetown, PE

Sheet Title
Overall Plan - Level 4 & Roof Plan

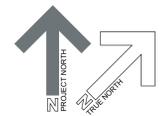
No.	Description	Date	Date:	Revision
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			Dim By: JLA	
			Project Number:	
			221079	
			Drawing Number:	
			A101	



1 ENLARGED PLAN - TIPPING HALL LEVEL 1
A102 1:100



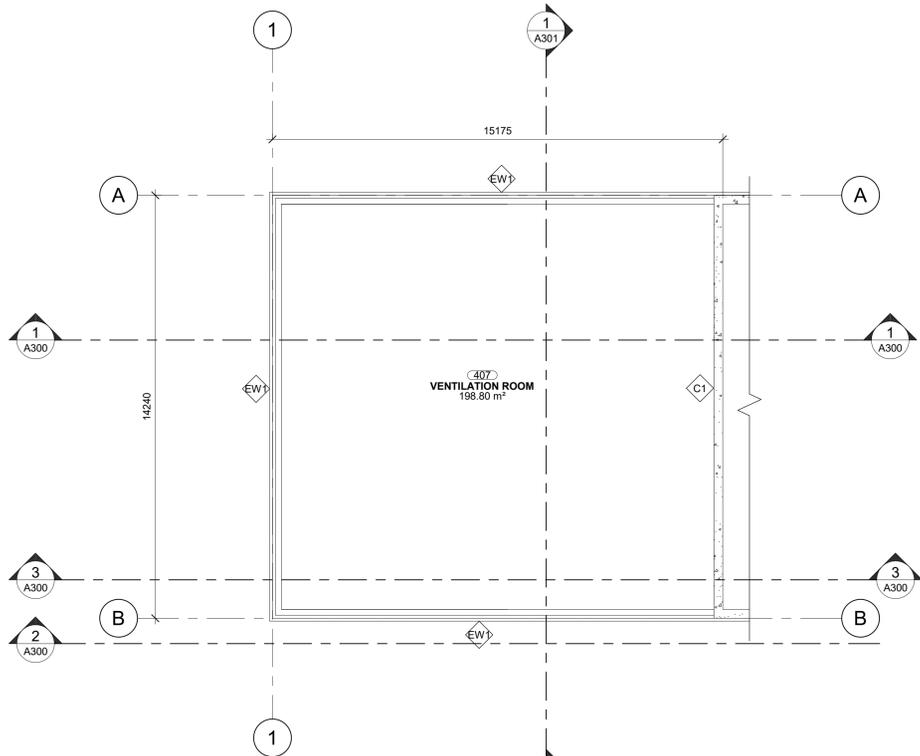
2 ENLARGED PLAN - LEVEL 1
A102 1:100



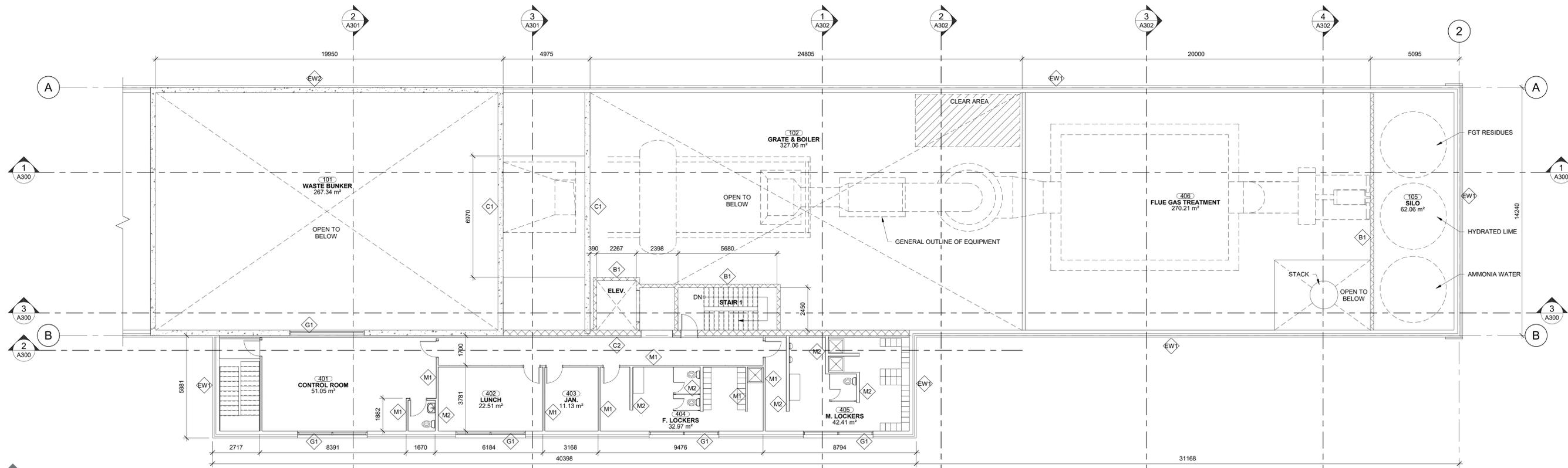
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					0	Issued for RFP	2022-11-18	<p>Drawn By: JLA Checked By: SDM Project Number: 221079 Drawing Number: A102</p>	0

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2 ENLARGED PLAN - TIPPING HALL LEVEL 4
1:100



1 ENLARGED PLAN - LEVEL 4
1:100

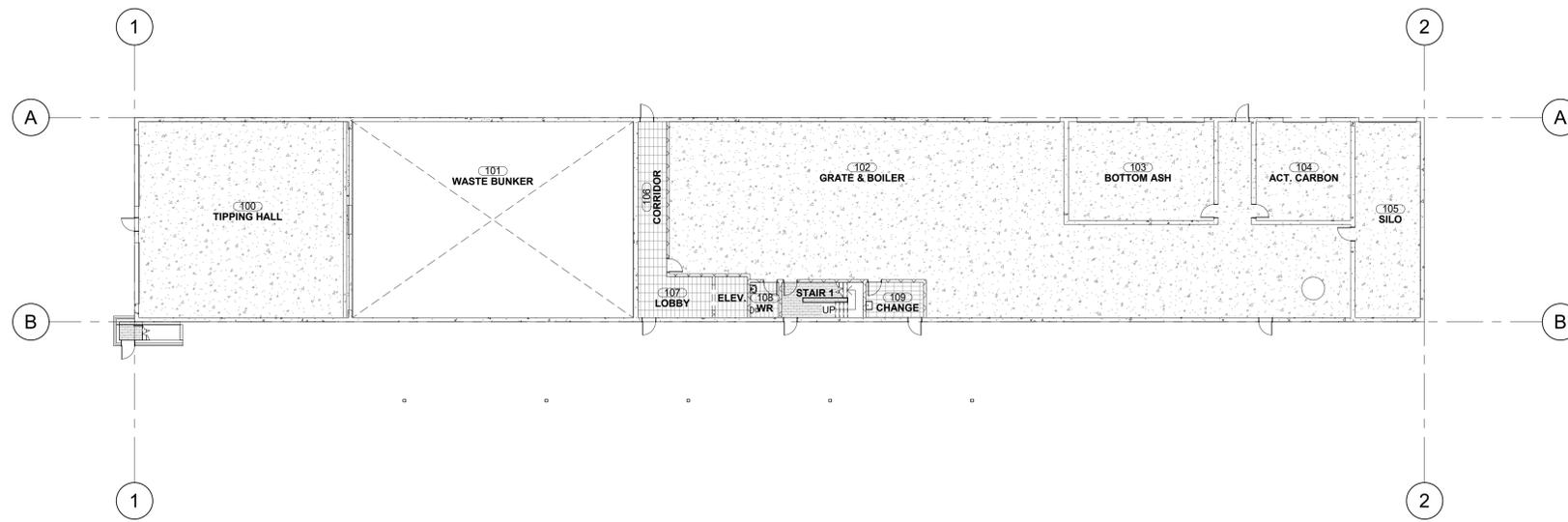


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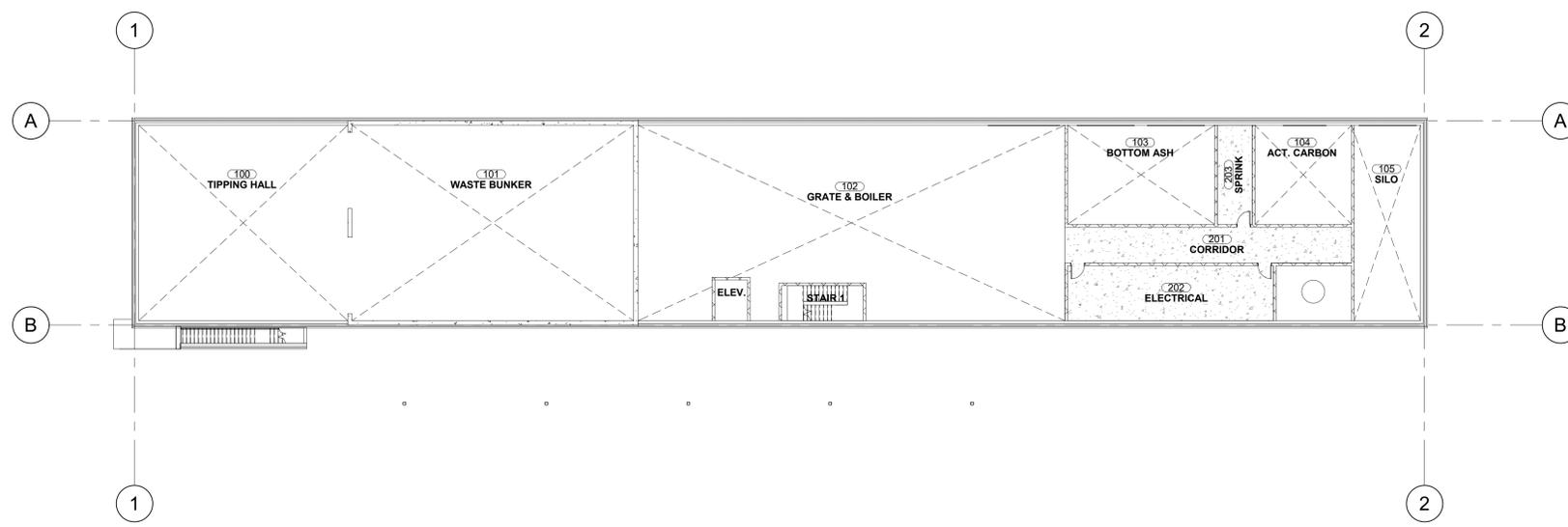
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				0	Issued for RFP	2022-11-18	<p>Drawn By: JLA</p> <p>Checked By: SDM</p> <p>Project Number: 221079</p> <p>Drawing Number: A105</p>	

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1 LEVEL 1 FINISH PLAN
A140 1:200



2 LEVEL 2 FINISH PLAN
A140 1:200

FLOOR FINISHES LEGEND	
	PORCELAIN FLOOR TILE, T-1
	VINYL COMPOSITE TILE, VCT-1
	EXPOSED CONCRETE WITH CLEAR SEALER, CO-1

NOTE:
STAIR 1 AND STAIR 2 ARE TO BE PROVIDED WITH RESILIENT STAIR TREADS, RISERS AND LANDINGS.



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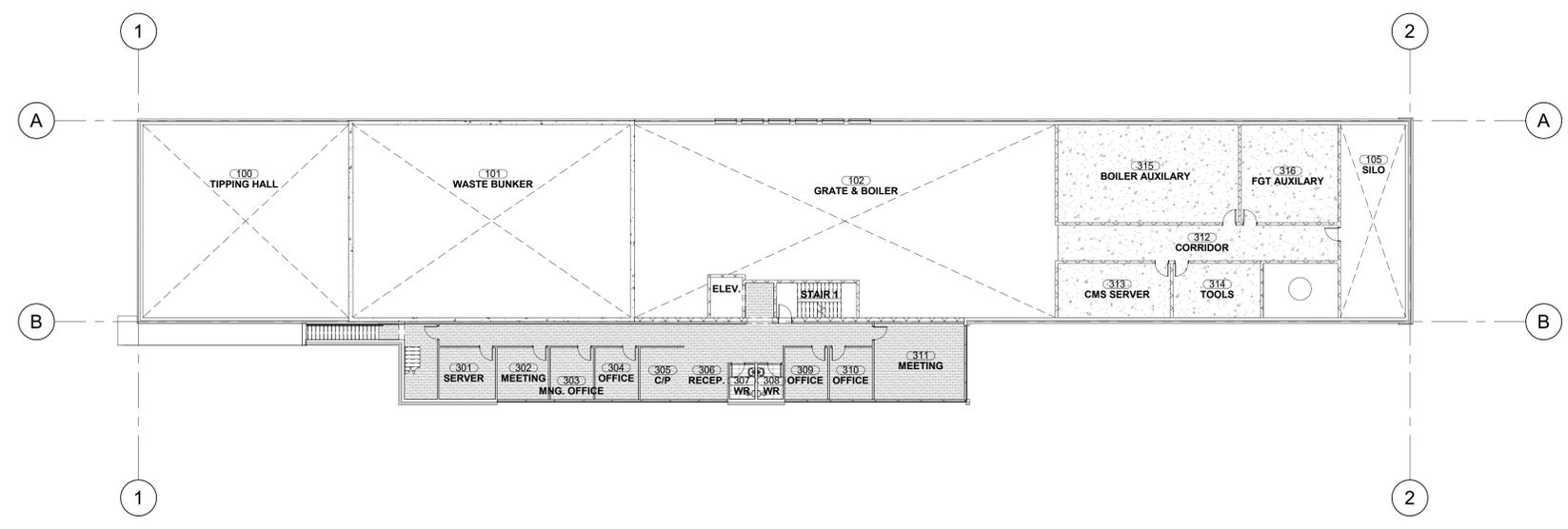
Client
PEI Energy Systems

Project Title
PEI Energy Systems Expansion
40 Riverside Drive
Charlottetown, PE

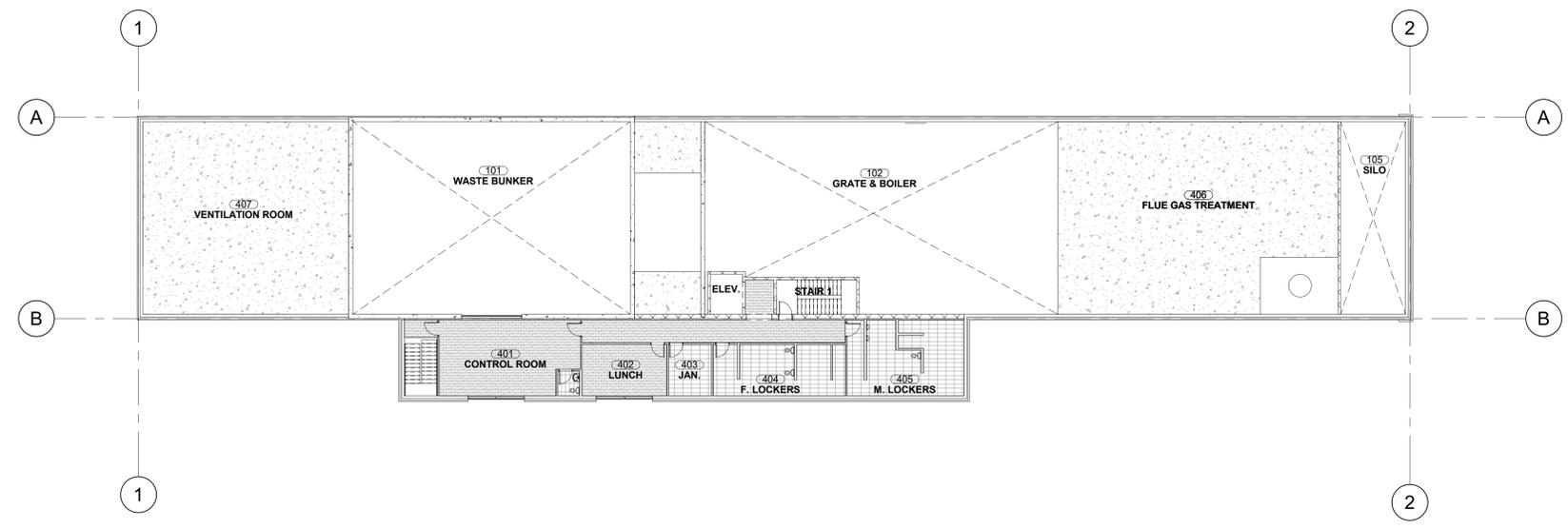
Sheet Title
Floor Finish Plan - Levels 1 & 2

No.	Description	Date	Date:	Revision
0	Issued for RFP	2022-11-18	2022-11-18	0
		Date: 2022-11-18		
		Dm By: JLA		
		Chk By: SDM		
		Project Number:		
		221079		
		Drawing Number:		
		A140		

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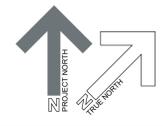
1
A141 LEVEL 3 FINISH PLAN
1:200



2
A141 LEVEL 4 FINISH PLAN
1:200

FLOOR FINISHES LEGEND	
	PORCELAIN FLOOR TILE, T-1
	VINYL COMPOSITE TILE, VCT-1
	EXPOSED CONCRETE WITH CLEAR SEALER, CO-1

NOTE:
STAIR 1 AND STAIR 2 ARE TO BE PROVIDED WITH RESILIENT STAIR TREADS, RISERS AND LANDINGS.



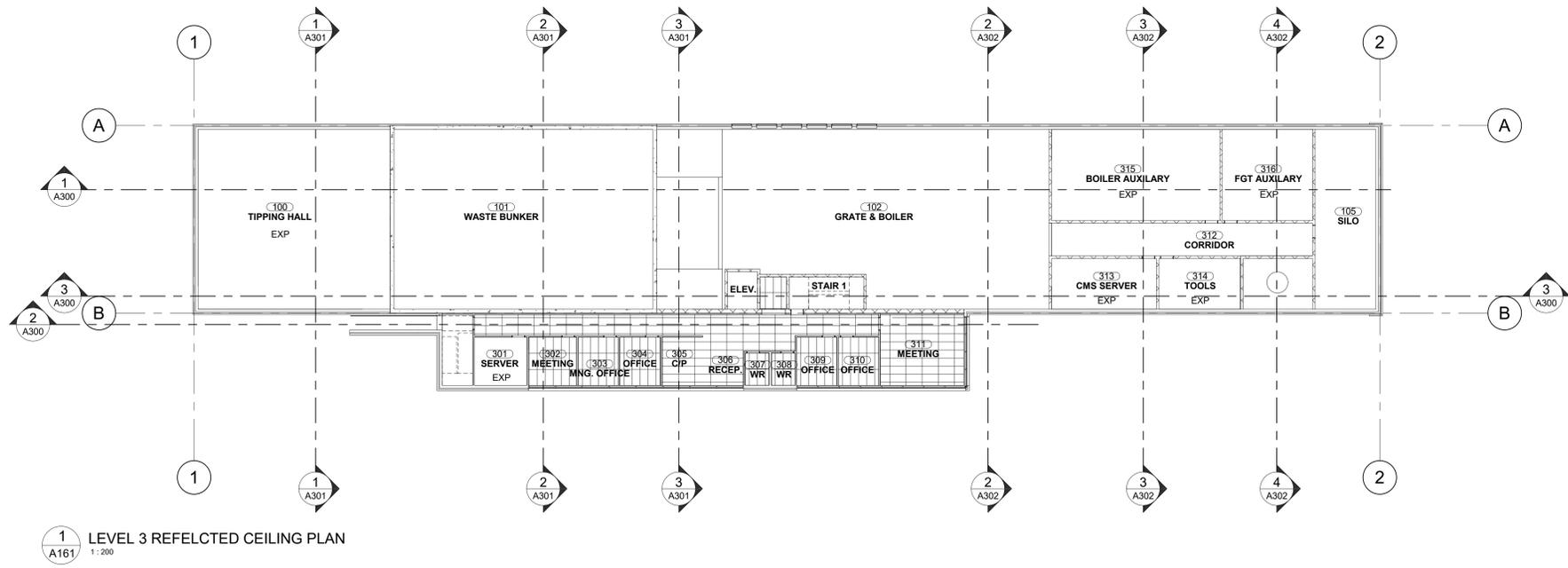
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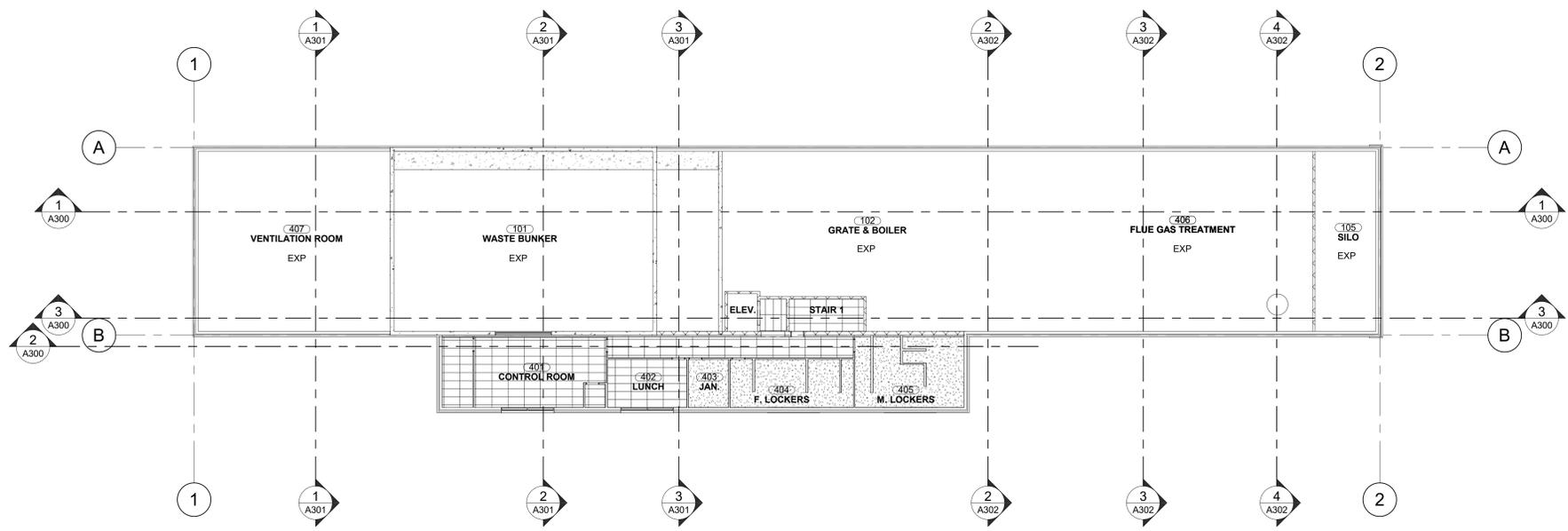
Project Title
PEI Energy Systems Expansion
40 Riverside Drive
Charlottetown, PE

Sheet Title
Floor Finish Plan - Levels 3 & 4

No.	Description	Date	Date:	Revision
0	Issued for RFP	2022-11-18	2022-11-18	0
			Dim By: JLA	
			Project Number: 221079	
			Drawing Number: A141	



1 LEVEL 3 REFLECTED CEILING PLAN
A161 1:200



2 LEVEL 4 REFLECTED CEILING PLAN
A161 1:200

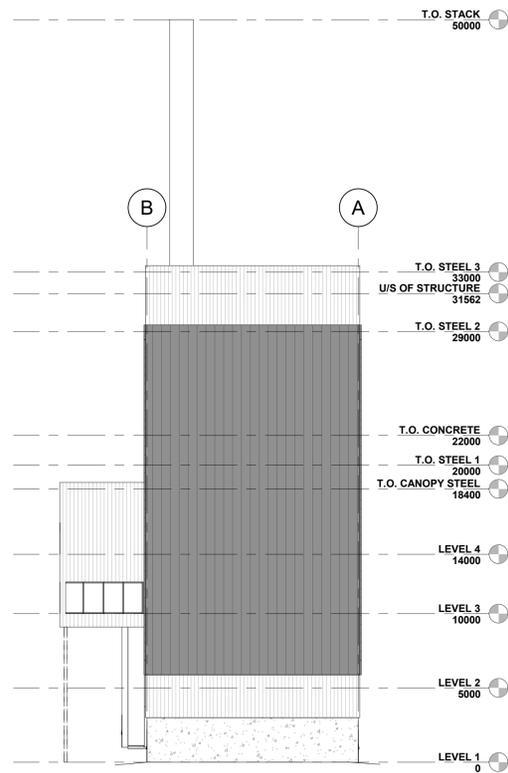
REFLECTED CEILING PLAN LEGEND	
	CEILING HEIGHT TAG, HEIGHTS TO BE DETERMINED BY DESIGN BUILDER
	GWB: 16mm TYPE X GWB ON METAL T-BAR SUSPENSION SYSTEM. SEE SPEC. HEIGHTS TO BE DETERMINED BY DESIGN BUILDER
	ACT-1: ACOUSTIC CEILING TILE, HEIGHTS AND SIZE OF TILE TO BE DETERMINED BY DESIGN BUILDER
	EXP: EXPOSED STRUCTURE. HEIGHTS TO BE DETERMINED BY DESIGN BUILDER

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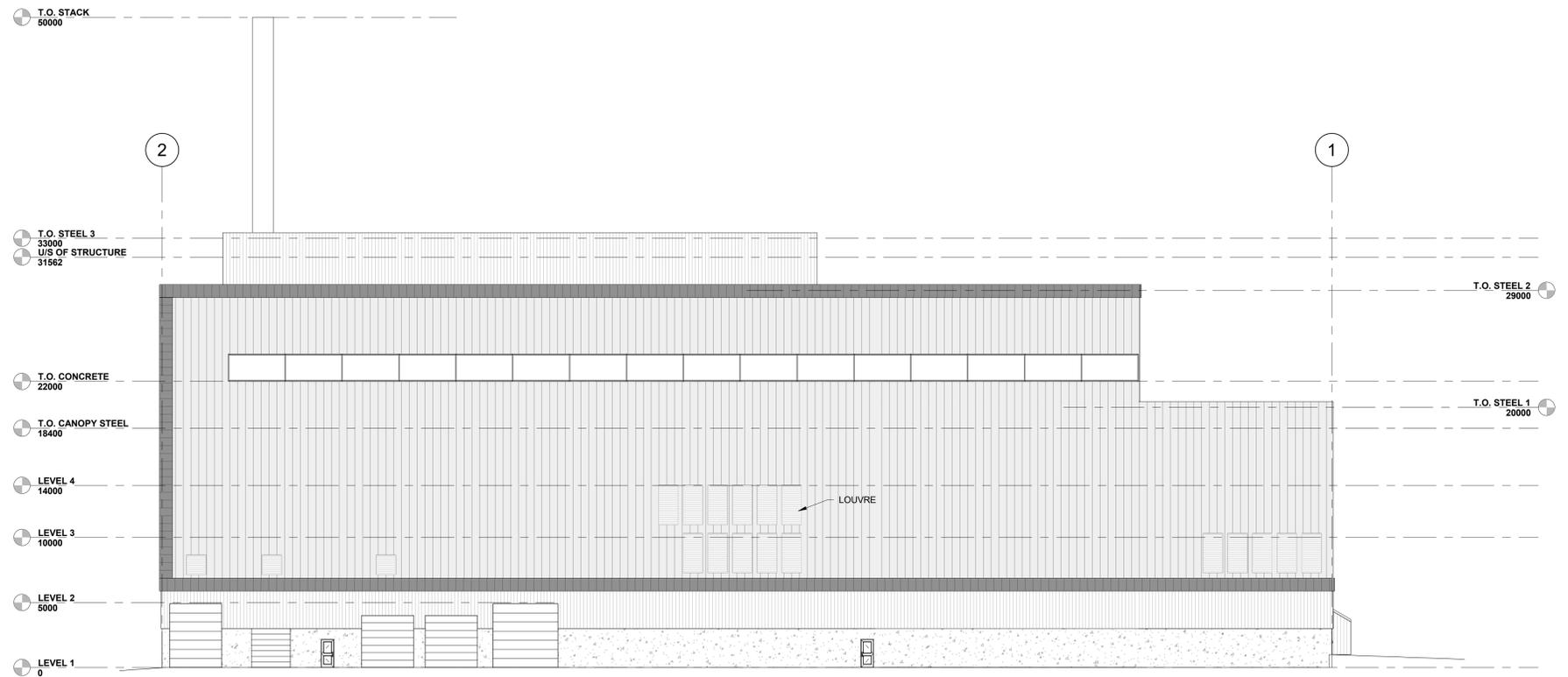


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				0	Issued for RFP	2022-11-18	<p>Drawn By: JLA</p> <p>Checked By: SDM</p> <p>Project Number: 221079</p> <p>Drawing Number: A161</p>	

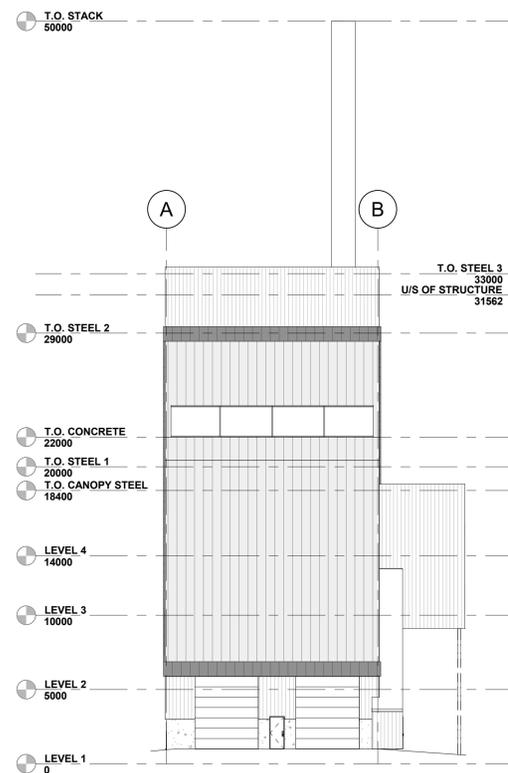
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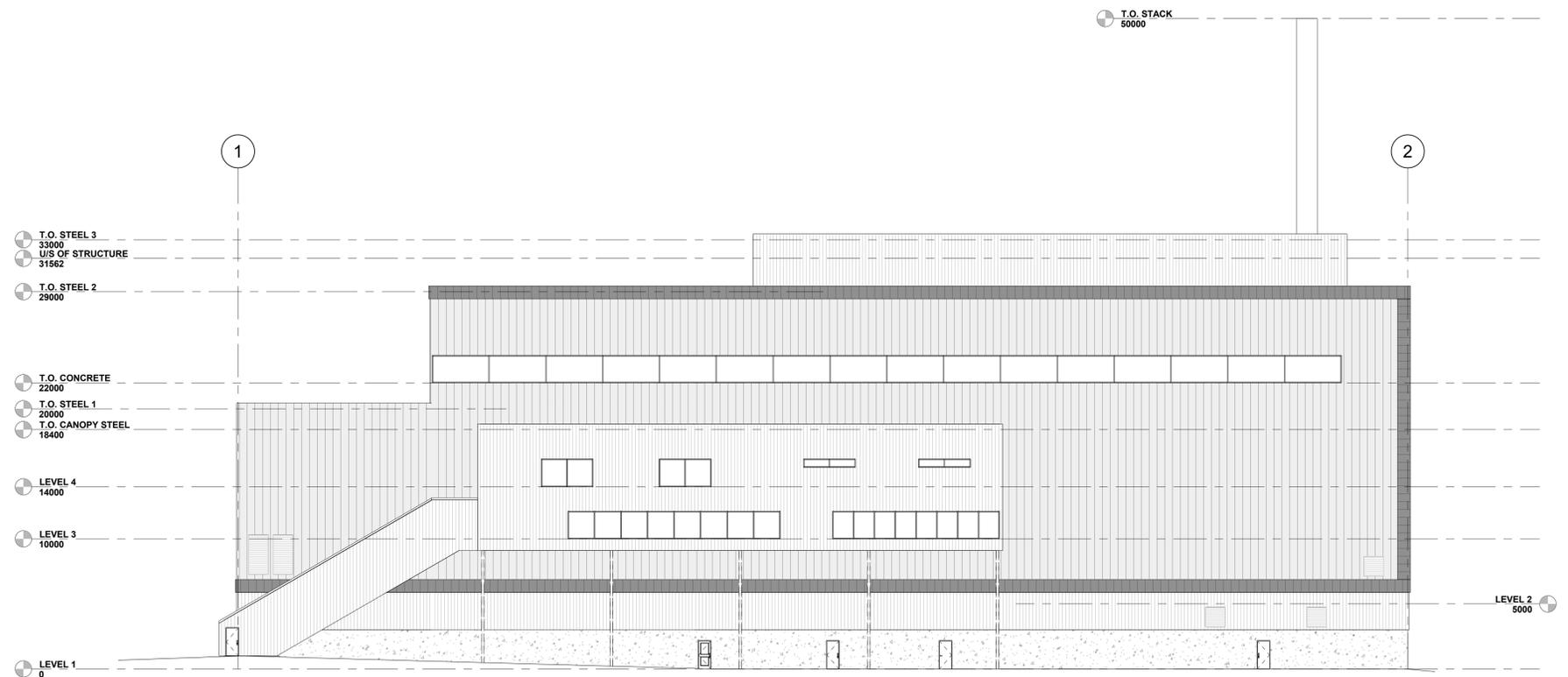
1 EAST ELEVATION
A200 1:200



2 NORTH ELEVATION
A200 1:200



3 WEST ELEVATION
A200 1:200



4 SOUTH ELEVATION
A200 1:200

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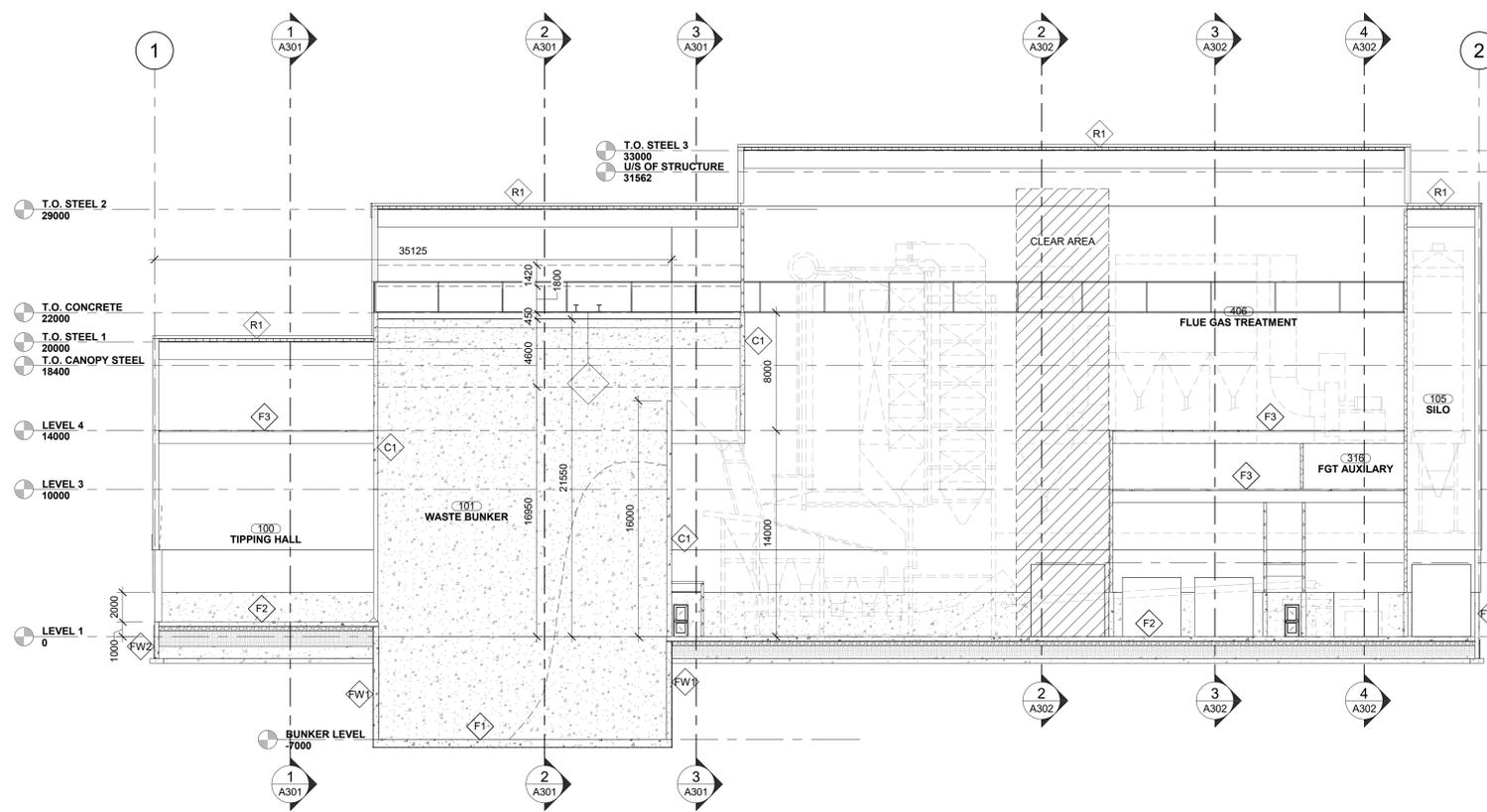
Suite 201, 85 Fitzroy Street
Charlottetown, P.E.I., Canada, C1A 1R6
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Client
PEI Energy Systems

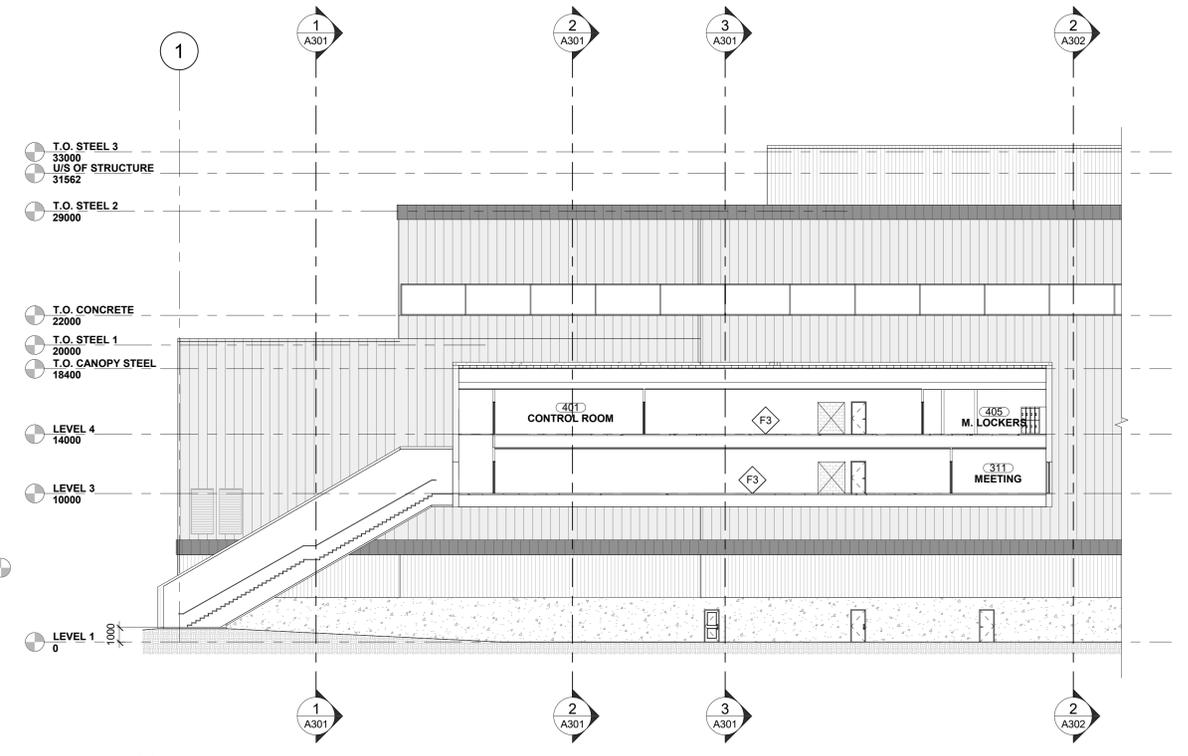
Project Title
PEI Energy Systems Expansion
40 Riverside Drive
Charlottetown, PE

Sheet Title
Exterior Elevations

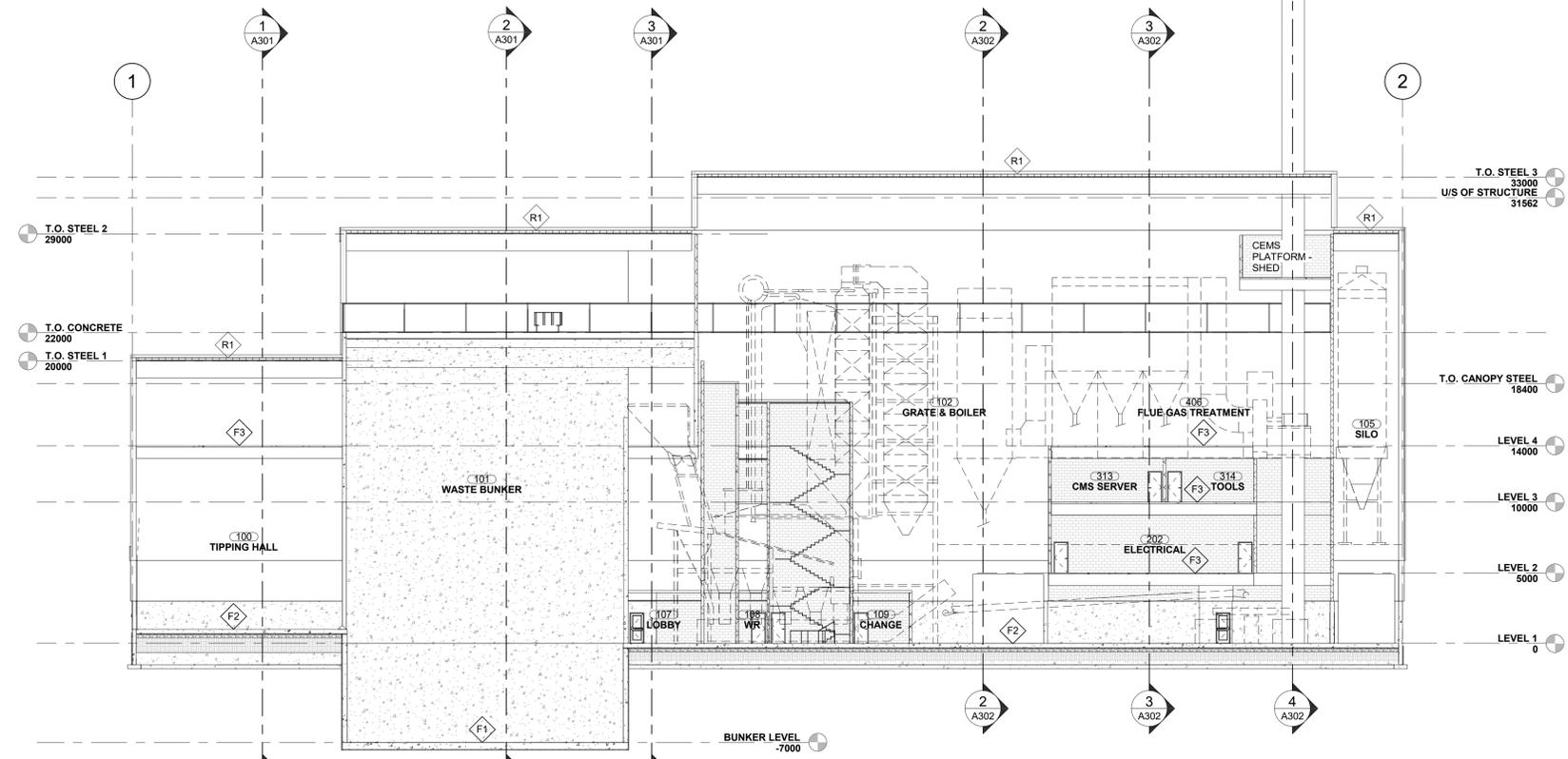
No.	Description	Date	Date:	Revision
0	Issued for RFP	2022-11-18	2022-11-18	0
			Chk By: JLA	
			Project Number:	
			221079	
			Drawing Number:	
			A200	



1 BUILDING SECTION
A300 1:200



2 BUILDING SECTION
A300 1:200

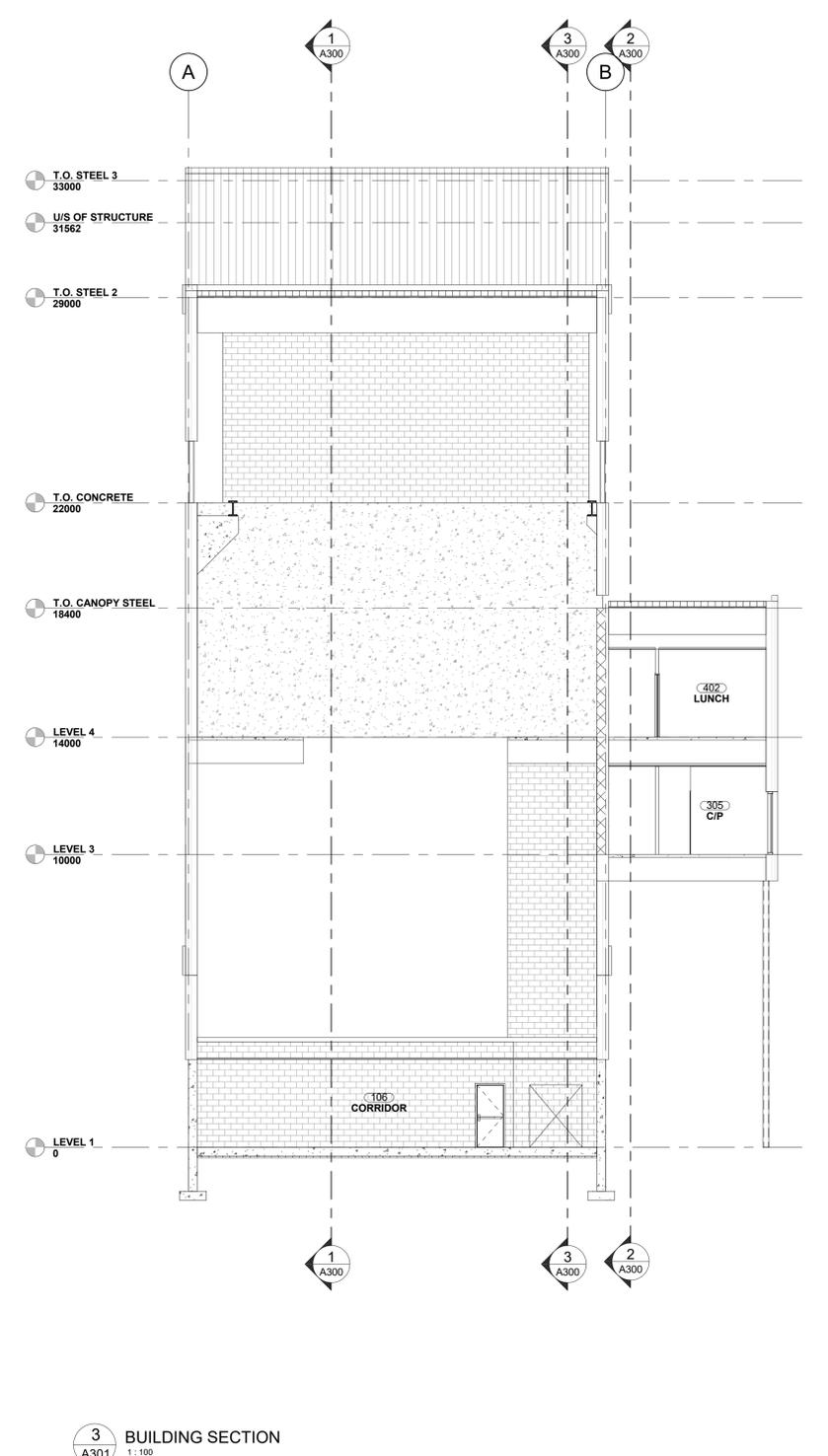
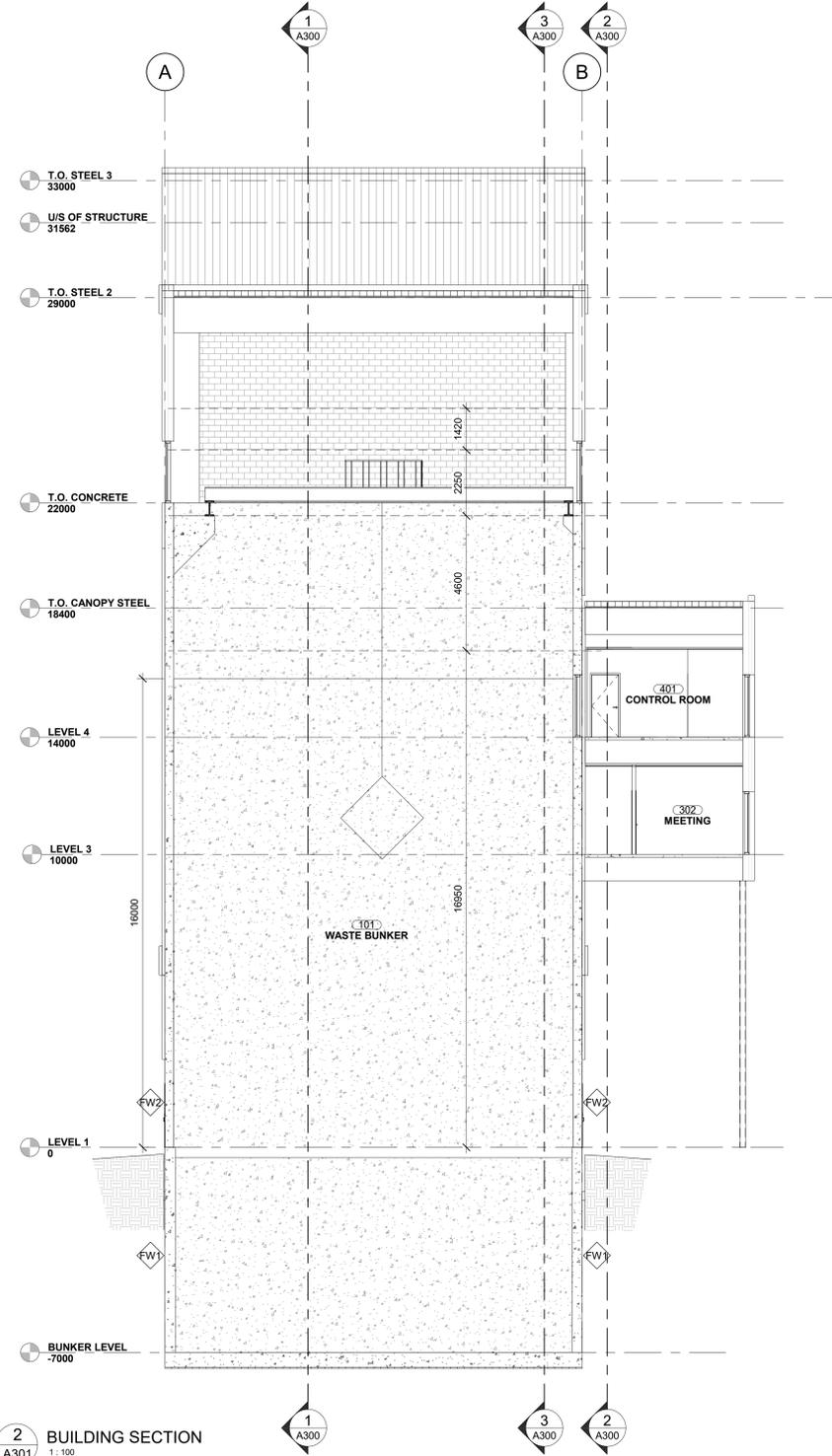
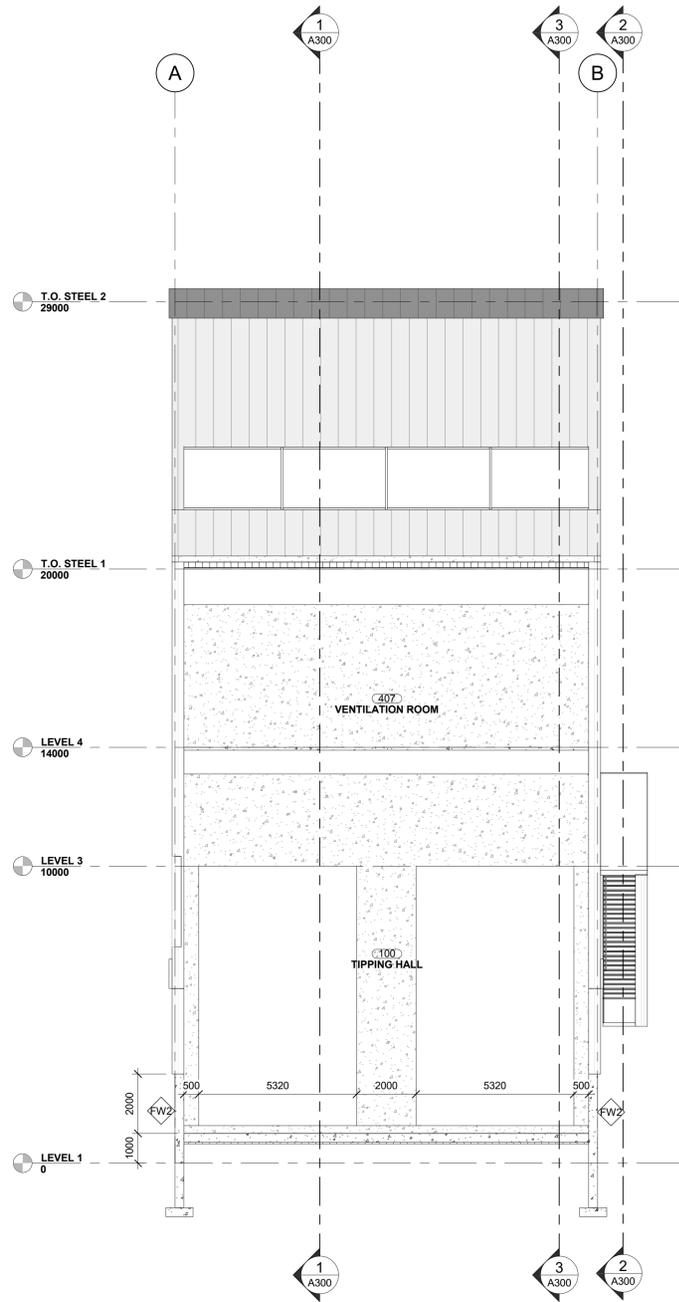


3 BUILDING SECTION
A300 1:200

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 <p>Suite 201, 85 Fitzroy Street Charlottetown, P.E.I., Canada, C1A 1R6 Phone: (802) 368-2300 www.colesassociates.com</p>	<p>Client PEI Energy Systems</p>	<p>Project Title PEI Energy Systems Expansion 40 Riverside Drive Charlottetown, PE</p>	<p>Sheet Title Building Sections</p>	No.	Description	Date	Date: 2022-11-18	<p>Revision 0</p>
				0	Issued for RFP	2022-11-18	<p>Drawn By: JLA Checked By: SDM Project Number: 221079 Drawing Number: A300</p>	

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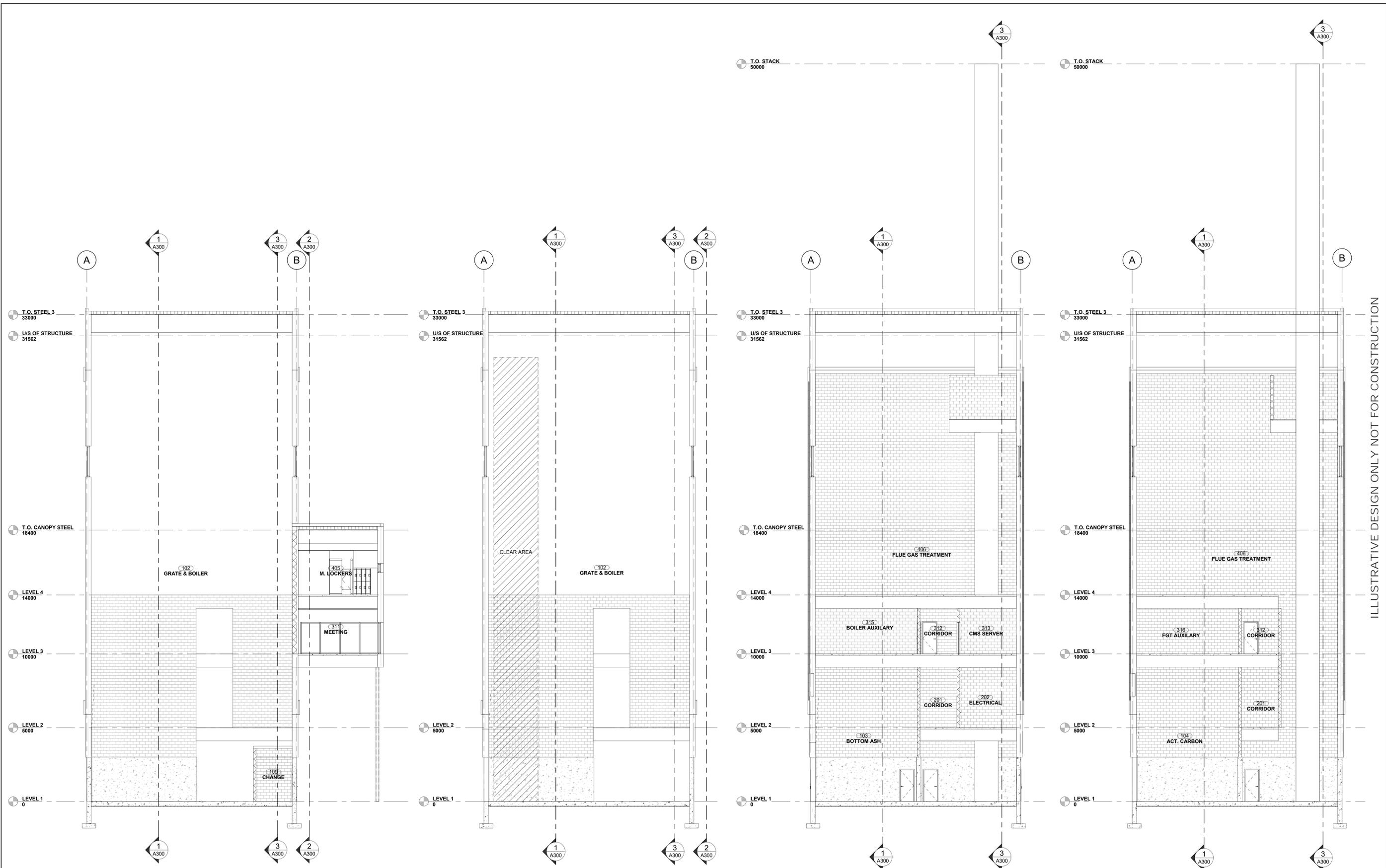
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Client
PEI Energy Systems

Project Title
PEI Energy Systems Expansion
40 Riverside Drive
Charlottetown, PE

Sheet Title
Building Sections

No.	Description	Date	Date:	Revision
0	Issued for RFP	2022-11-18	2022-11-18	0
			Drawn By: JLA	
			Chk By: SDM	
			Project Number:	
			221079	
			Drawing Number:	
			A301	



1 BUILDING SECTION
A302 1:100

2 BUILDING SECTION
A302 1:100

3 BUILDING SECTION
A302 1:100

4 BUILDING SECTION
A302 1:100

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Client
PEI Energy Systems

Project Title
PEI Energy Systems Expansion
40 Riverside Drive
Charlottetown, PE

Sheet Title
Building Sections

No.	Description	Date	Date:	Revision
0	Issued for RFP	2022-11-18	2022-11-18	0
			Chk By: JLA	
			Project Number:	
			221079	
			Drawing Number:	
			A302	

Attachment B

External Design Reviewer's Comments (June 8, 2023)

File: DESIGN-2023-30-OCTOBER-6C
40 Riverside Drive (PID #839332)
Applicant: Nazmi Lawen
Owner: Thunderbolt Energy Assets GP Inc


CHARLOTTETOWN
Planning & Heritage
Department



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08 June 2023

Robert Zilke, MPlan, RPP, MCIP
Planner II – Planning and Heritage Department
70 Kent St.
City of Charlottetown, PE
C1A 1M9

Concept Review
40 Riverside Dr

The site is an important one. It is highly visible from the Route 1 causeway as well from Stratford and therefore, requires careful design development to enhance the location.

The suggested massing and detail suggest a contemporary solution to an industrial building. The linear windows on the upper level of the South façade are nice and will add visual interest to the façade both during the day and night. The exterior appears to be a clean minimalist aesthetic. If it evolves to become the preferred approach, it shouldn't become cluttered with "engineering" grilles, vents, pipes, etc. There is an office component on piloti which I think may be an overhang on a drive aisle. The size of the windows and placement is nice, and I believe as the design evolves can play a significant role in the façade of this element.

I'm having difficulty in determining the staff/main entrance to the building. There is a long "diagonal" stair leading from the ground to the Level 3. If the diagonal stair is simply an exit, then consideration should be given to a more compact shape and perhaps combined with the more conventional internal exit office stair. If this is not possible, then consideration should be given to a more graceful structure. The elevator is tucked further into the footprint and adjacent to a lobby with a cross corridor. Emphases need to be given to this south "main" entrance. I realize it's internal to the site [behind the existing building], but it should be made visually more interesting.

As mentioned above, the site is quite important as it has exposure from three directions: east and west on Highway 1 and from the south across the water. From these views, attention needs to be made to how the building will read from these perspectives.

There can be several design vocabularies.

One being a very elegant building [as presented] with subtle use of color or colour shades to accentuate the mass and volumes. This should also address the base of the facility.

Another scenario is to look at all facades as a canvas and for the proponent to create a visually stimulating façade. A couple of examples might be: -

- in the cladding, incorporate a geometric pattern - Mondrian, or Francois Morellet as inspiration. Reference the graphics on the fly tower of the Fredericton Playhouse,
- projected images, either as still life or a video projection, such as in Quebec City where there is a long bank of grain silos that are illuminated at night with colour changing LED lights.

PARTNERS:

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Please keep in mind that all of the building elevations are equal, highly visible, and therefore similar consideration should be given to their treatment.

In any scenario thought should be given to the size, placement and geometry of the fenestration, overhead doors, and other means of access.

Finally, the site needs to be addressed in a thoughtful way. In looking at google earth the site needs organization to reduce the amount of asphalt, attention to the need and placement of the miscellaneous small structures and general onsite storage and screening of equipment. The site plan, as presented suggests simply adding the building and extending the property into the basin.

The city's official plan addresses a pedestrian link that would extend from along the west waterfront across this site and over to the Queen Elizabeth Hospital property. Now is the time to allow for this pathway.

With the encroachment of the site into the waterfront, the shoreline has been reinforced with rip rap. The importance of this is not lost, but a more environmentally sound solution needs to be incorporated by the greening of the design. With the relocation of the hot water silo, there is an opportunity to reduce the heat island effect of the yard whilst maintaining the truck movements and separating the small vehicle parking, from the heavy vehicles. The rationalized space would be landscaped which would help address the view of the existing facility as seen from the street and pedestrian pathway.

The proponent should demonstrate the vehicle movements to help determine how the site is used. There are creative solutions to storm water management which could be considered. The bonus would be the greening of the site which builds on the environmental "spin" of the facility, whilst improving the aesthetics as seen from the causeway, Stratford and providing a buffer to the proposed pedestrian pathway.

Overall, the project has nice massing, but the next iteration needs to take the design to the next level where more of the building design and urban character needs to be explored.

Prepared by: -
N45 Architecture Inc.

A blue ink handwritten signature, appearing to read 'Robert Matthews', is written over the printed name below.

Robert Matthews

Attachment C

Updated Site Plan and 3D Concepts (July 27, 2023)

File: DESIGN-2023-30-OCTOBER-6C
40 Riverside Drive (PID #839332)
Applicant: Nazmi Lawen
Owner: Thunderbolt Energy Assets GP Inc



NEW SHRUBS, TYPE TBD
 NEW TREES SPACED OUT 20'
 TYPE TBD



RIVERSIDE DRIVE

GENERAL NOTE:
 ALL EXISTING TREES TO REMAIN

1
 A001
 NTS
 VIEW OF BERM ALONG RIVERSIDE DRIVE LOOKING EAST

NEW SHRUBS, TYPE TBD
 NEW TREES SPACED OUT 20'
 TYPE TBD



← TO QEH RIVERSIDE DRIVE TO DOWNTOWN CHARLOTTETOWN →

2
 A001
 NTS
 VIEW OF BERM ALONG RIVERSIDE DRIVE LOOKING SOUTHEAST



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 www.colesassociates.com

Client
 PEI Energy Systems

Project Title
 PEI Energy Systems Expansion
 40 Riverside Drive
 Charlottetown, PE

Sheet Title
 3D Views
 Site Landscape Concepts

No.	Description	Date	Date: 2023-07-26	Revision
			Dm By: JJJ/GS	3
			Chk By: SDM	
			Project Number:	
			221079	
			Drawing Number:	
			A001	

Attachment D

External Design Reviewer's Comments (July 31, 2023)

File: DESIGN-2023-30-OCTOBER-6C
40 Riverside Drive (PID #839332)
Applicant: Nazmi Lawen
Owner: Thunderbolt Energy Assets GP Inc


CHARLOTTETOWN
Planning & Heritage
Department



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31 July 2023

Robert Zilke, MPlan, RPP, MCIP
Planner II – Planning and Heritage Department
70 Kent St.
City of Charlottetown, PE
C1A 1M9

Concept Review
40 Riverside Dr

The current site plan submission has not addressed the site in any meaningful way as discussed in my review of 08 June.

I would encourage the proponent to engage a landscape architect to assist in the design of the site.

A number of suggestions were made in the June letter, as follows:-

1. **Reduce the amount of asphalt** – The current design has not addressed this. The truck movements indicate that the asphalt could be reduced extensively.
2. **Miscellaneous small structures and on-site storage** - Street view showed small structures that are unsightly. The final approved site plan should address on site storage. If it cannot be accommodated in the new building, then a more permanent solution needs to be considered – either a small structure or a designed screened enclosure.
3. **Hot water Tank** – the site does not show the location of the “relocated hot water tank.” This will affect the site redevelopment.
4. **Heat island effect** – in order to reduce this and to increase the landscaped area, the hot water tank location needs to be sited and the truck and car movements better planned.

PARTNERS:

ROBERT MATTHEWS
B.Arch., OAA
AAPEI, FRAIC

VLADIMIR POPOVIC
OAA, AAPEI, FRAIC
LEED ap bd+c

NATHALIE ROUTHIER
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MRAIC, LEED ap bd+c

SENIOR ASSOCIATES:

GERRY MALLETTE
Dipl. Architectural Technology

KEITH DICKIE
B. Arch, OAA, MRAIC

The plan as submitted doesn't address any of the original concerns other than adding "New trees spaced out 20' TBD".

Another submission is required addressing the above comments.

Prepared by: -
N45 Architecture Inc.

A handwritten signature in blue ink, appearing to read "Robert Matthews", is written over a blue circular stamp or seal. Below the signature, the name "Robert Matthews" is printed in a standard black font.

Robert Matthews

Attachment E

Updated Site Plan (August 16, 2023)

File: DESIGN-2023-30-OCTOBER-6C
40 Riverside Drive (PID #839332)
Applicant: Nazmi Lawen
Owner: Thunderbolt Energy Assets GP Inc



Attachment F

Response from Enwave (July 27- August 24, 2023)

File: DESIGN-2023-30-OCTOBER-6C
40 Riverside Drive (PID #839332)
Applicant: Nazmi Lawen
Owner: Thunderbolt Energy Assets GP Inc



Kamal, Sonia

From: Zilke, Robert <rzilke@charlottetown.ca>
Sent: August 24, 2023 3:31 PM
To: Chris MacDougall
Cc: Trainor, Emily
Subject: RE: 221079 Enwave / 40 Riverside Drive - Updated Site Plan and 3D COncpts

Caution: This email originated from outside the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Chris,

This application does not have to go back to Planning Board. Once the next Design Review Board meeting's date is determined our office will inform you. The timeline for the south-west corner is fairly long but since these landscaping requirements are part of the design reviewer recommendation and if supported by the Design Review Board it would be a condition on the building permit for the construction of the new facility.

This landscaping work would need to be completed prior to issuance of the occupancy for the proposed new waste to energy building.

Let me know if Let me know if you have any questions.

Robert

From: Chris MacDougall <Chris.MacDougall@enwave.com>
Sent: Monday, August 21, 2023 12:11 PM
To: Zilke, Robert <rzilke@charlottetown.ca>; Trainor, Emily <etrainor@charlottetown.ca>
Subject: RE: 221079 Enwave / 40 Riverside Drive - Updated Site Plan and 3D COncpts

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Hi Robert.

Just checking in again on this. I see a Planning Board Meeting scheduled for September 5 and I believe you want this to go to Design Review Before that but I don't see it scheduled yet?

Can you advise

Chris

From: Chris MacDougall
Sent: Wednesday, August 16, 2023 3:43 PM
To: Zilke, Robert <rzilke@charlottetown.ca>; JV Javier <jvjavier@colesassociates.com>; Scott MacNeill <smacneill@colesassociates.com>

Cc: Douglas Coles <dcoles@caltech.ca>; Nazmi Lawen <nlawen@colesassociates.com>; Gurwinder Singh <gsingh@colesassociates.com>; Marcus King <mking@colesassociates.com>; Trainor, Emily <etrainor@charlottetown.ca>

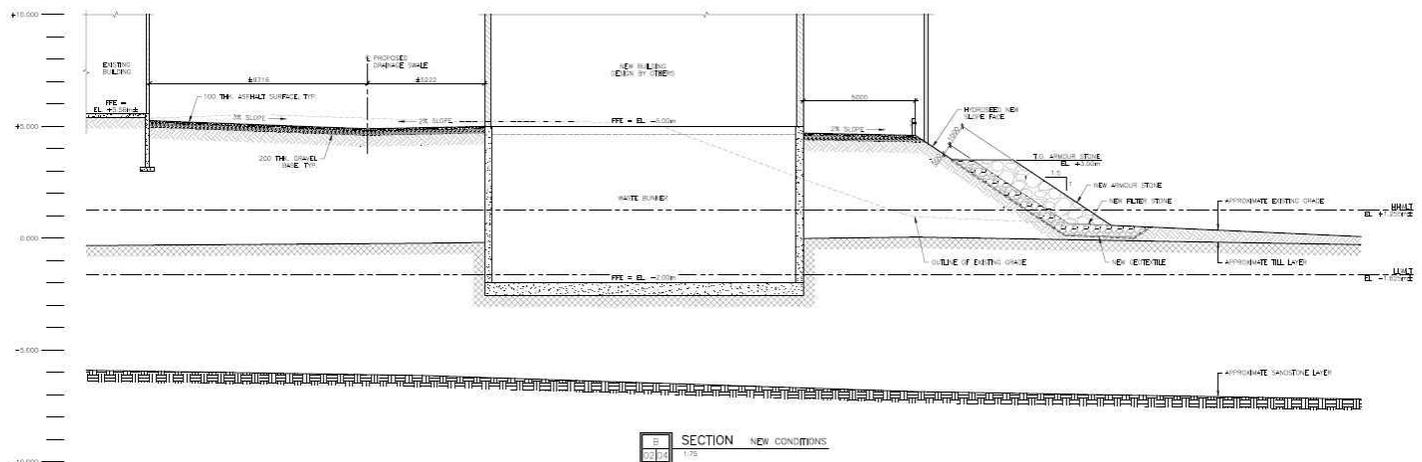
Subject: RE: 221079 Enwave / 40 Riverside Drive - Updated Site Plan and 3D COncepts

Hi Robert;

For the Hillsborough River boundary of the EFW Renewal project there is an approved alteration permit which includes an infill, armoring/seawall. See proposed profile below. A naturalized bank would not meet the demands required. This is for bank protection and climate resiliency (PEI 2050 sea level rise, storm surge, wave action etc. to prevent flooding and property destruction). There is nothing special about this area. However as part of the process PEIES has contracted with DFO to enhance an offsite section upstream in the East Hillsborough River. In brief, the river enhancement project will create a new reef where 25,000 juvenile oysters will seeded. This project took about a year to plan and approve in consultation with DFO, Indigenous First Nations and the PEI Shellfish Association. The oysters help filter algae and improve overloaded nutrients in the river as well as provide spat for more oysters. The reef creates a habitat for other aquatic life.

I don't have a feasible plan for the south-west corner or the timelines as yet. In 3 months we would plan to have our EPC contracts in place followed by 6 months of design. I think I would have a solid idea of site demands in April. If that is too long of a timeline let me know.

Chris



From: Zilke, Robert <rzilke@charlottetown.ca>

Sent: Wednesday, August 16, 2023 1:16 PM

To: Chris MacDougall <Chris.MacDougall@enwave.com>; JV Javier <jjvavier@colesassociates.com>; Scott MacNeill <smacneill@colesassociates.com>

Cc: Douglas Coles <dcoles@caltech.ca>; Nazmi Lawen <nlawen@colesassociates.com>; Gurwinder Singh <gsingh@colesassociates.com>; Marcus King <mking@colesassociates.com>; Trainor, Emily <etrainor@charlottetown.ca>

Subject: RE: 221079 Enwave / 40 Riverside Drive - Updated Site Plan and 3D COncepts

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Hello Chris,

Thanks for the updated site plan, could you provide clarification on the proposed bank enhancement that your partnering with DFO? Specifically, is this a naturalized bank? Do you have cross sections of the proposed work? Also, what is the plan and timeframe for the greenspace opportunity in the south-west corner by the entrance?

I think we are almost there, I just need some specificity concerning the proposed green enhancements so I can tie it to the issuance of a building and development permit.

Thanks,

Robert

From: Chris MacDougall <Chris.MacDougall@enwave.com>
Sent: Tuesday, August 15, 2023 1:35 PM
To: Zilke, Robert <rzilke@charlottetown.ca>; JV Javier <jjavier@colesassociates.com>; Scott MacNeill <smacneill@colesassociates.com>
Cc: Douglas Coles <dcoles@caltech.ca>; Nazmi Lawen <nlawen@colesassociates.com>; Gurwinder Singh <gsingh@colesassociates.com>; Marcus King <mking@colesassociates.com>; Trainor, Emily <etrainor@charlottetown.ca>
Subject: RE: 221079 Enwave / 40 Riverside Drive - Updated Site Plan and 3D COncepts
Importance: High

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Robert,

Attached is the site plan with some markups that I have made from notes that didn't make it into the last site plan remitted to Robert Matthews. Ideally I wish to finalize this file at the next meeting.

- The team did review any opportunities to reduce the asphalt. As this is a EPC contract not all future operational requirements are known. I have made one area available to be determined. Keep in mind not all traffic was included in the site plan either. This is a very busy site and we lease this land from the Provincial Government. We may have an opportunity to reduce the asphalt on the S-E side as well once all equipment and emergency access requirements are also known.

In addition I have marked out areas where additional trees are to be reinstated or newly planted. There is an area marked out for perennial / annual flower beds.

- PEI Energy Systems is committed to improving the street view of the existing plant and small structures and on-site storage. This includes; improved and/ or additional fencing, moving storage inside where appropriate, repainting the existing building.
- The hot water storage tank will ideally go back near to the original location once IWMC and other waste haulers advise on their planned vehicles and turning radius' are known.
- The heat island effect is noted. It is also noted that the City approves building permits for parking lots every month without this consideration. However PEI Energy Systems has made a substantial contribution to the environment, GHG savings and improved river frontage offsetting program. A marine enhancement program in the East Hillsborough River has been approved with PEI Dept. of Environment and Federal DFO. The site has hosted the Canadian Federation of Municipalities several times and has been commended on the additional public value to the City.

Sincerely submitted,

Chris MacDougall

Operations Engineer

PEI Energy Systems

T: (902) 629-3960 x-230 | M: (902) 394-4626

E: Chris.MacDougall@Enwave.com

40 Riverside Drive

Charlottetown, PEI

Canada, C1A 9M2

www.enwave.com



From: Zilke, Robert <rzilke@charlottetown.ca>

Sent: Tuesday, August 1, 2023 3:24 PM

To: JV Javier <vjavier@colesassociates.com>; Chris MacDougall <Chris.MacDougall@enwave.com>; Scott MacNeill <smacneill@colesassociates.com>

Cc: Douglas Coles <dcoles@caltech.ca>; Nazmi Lawen <nlawen@colesassociates.com>; Gurwinder Singh <g Singh@colesassociates.com>; Marcus King <mking@colesassociates.com>; Trainor, Emily <etrainor@charlottetown.ca>

Subject: RE: 221079 Enwave / 40 Riverside Drive - Updated Site Plan and 3D COncepts

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Hello,

Mr. Matthews responded to the latest iteration of the site plan and noted various items that were not addressed. See attached his letter that provides an overview of items that were never addressed in your resubmission.

Prior to resubmitting another concept, please address the recommendations outlined in Mr. Matthew's latest letter that is attached.

Thanks,

Robert

From: JV Javier <vjavier@colesassociates.com>

Sent: Thursday, July 27, 2023 12:55 PM

To: Zilke, Robert <rzilke@charlottetown.ca>

Cc: Chris MacDougall <Chris.MacDougall@enwave.com>; Scott MacNeill <smacneill@colesassociates.com>; Douglas Coles <dcoles@caltech.ca>; Nazmi Lawen <nlawen@colesassociates.com>; Gurwinder Singh <g Singh@colesassociates.com>; Marcus King <mking@colesassociates.com>; Trainor, Emily <etrainor@charlottetown.ca>

Subject: 221079 Enwave / 40 Riverside Drive - Updated Site Plan and 3D COncepts

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good day Robert.

Attached are the updated Site Plan and 3D Site Concepts addressing some of Robert Matthews review notes.

Do let us know if you need anything else.

Regards.

JV Javier

Senior Architectural Designer



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Attachment G

External Design Reviewer's Comments (August 25, 2023)

File: DESIGN-2023-30-OCTOBER-6C
40 Riverside Drive (PID #839332)
Applicant: Nazmi Lawen
Owner: Thunderbolt Energy Assets GP Inc


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25 August 2023

Robert Zilke, MPlan, RPP, MCIP
Planner II – Planning and Heritage Department
70 Kent St.
City of Charlottetown, PE
C1A 1M9

Concept Review
40 Riverside Dr
Revised Site Plan 16 August 2023

I have reviewed the above submission and appreciate the situation with respect to land ownership and the unknown future development plans. With this in mind, I find the proposed conceptual landscaping to be acceptable. I would recommend that the City work with the applicant's landscape architect to ensure that the areas highlighted in green are treated in an appropriate manner.

Prepared by: -
N45 Architecture Inc.

A handwritten signature in blue ink, appearing to read "Robert Matthews", is written over a large, stylized blue scribble that partially obscures the text above it.

Robert Matthews

PARTNERS:

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AAPEI, FRAIC

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NATHALIE ROUTHIER
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Dipl. Architectural Technology

KEITH DICKIE
B. Arch, OAA, MRAIC

Attachment H



File: DESIGN-2023-30-OCTOBER-6C
40 Riverside Drive (PID #839332)
Applicant: Nazmi Lawen
Owner: Thunderbolt Energy Assets GP Inc

